

CITY OF



AGOURA HILLS

Agoura Village Specific Plan

October 22, 2008



Agoura Village Specific Plan

Adopted October 22, 2008

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CHAPTER 1: INTRODUCTION

Introduction

The Agoura Village Specific Plan (the Plan) is the result of a long range planning effort launched by the City of Agoura Hills that will lead to the revitalization, the appropriate use, and the beautification of the Agoura Village (the Village) area. The Specific Plan area is located about half way between Westlake Village and Calabasas just off of Highway 101 within the City of Agoura Hills. The Specific Plan area runs along both sides of Agoura Road, one block west of Kanan Road to approximately two blocks east of Cornell Road. Roadside Drive and Highway 101 border much of the area to the north and open space borders the south (Refer to Figure 1.1 - Context Map.) The Agoura Village area is presently characterized by a mix of land uses and parcels of various shapes and sizes—some viable, some marginal—but with little sense of place and cohesion.

Background

In 1997 - 1998, Agoura Village was the subject of a “design brainstorming” effort lead by the City and a consulting team that resulted in a “Village Concept Plan” embracing a pedestrian-friendly, vibrant, and entertainment-oriented village scale vision for the area. To support this vision, a special overlay Zone (Agoura Village Overlay District—AV) was developed for the Village as an interim solution to prevent inappropriate uses from developing until a detailed plan could be developed.

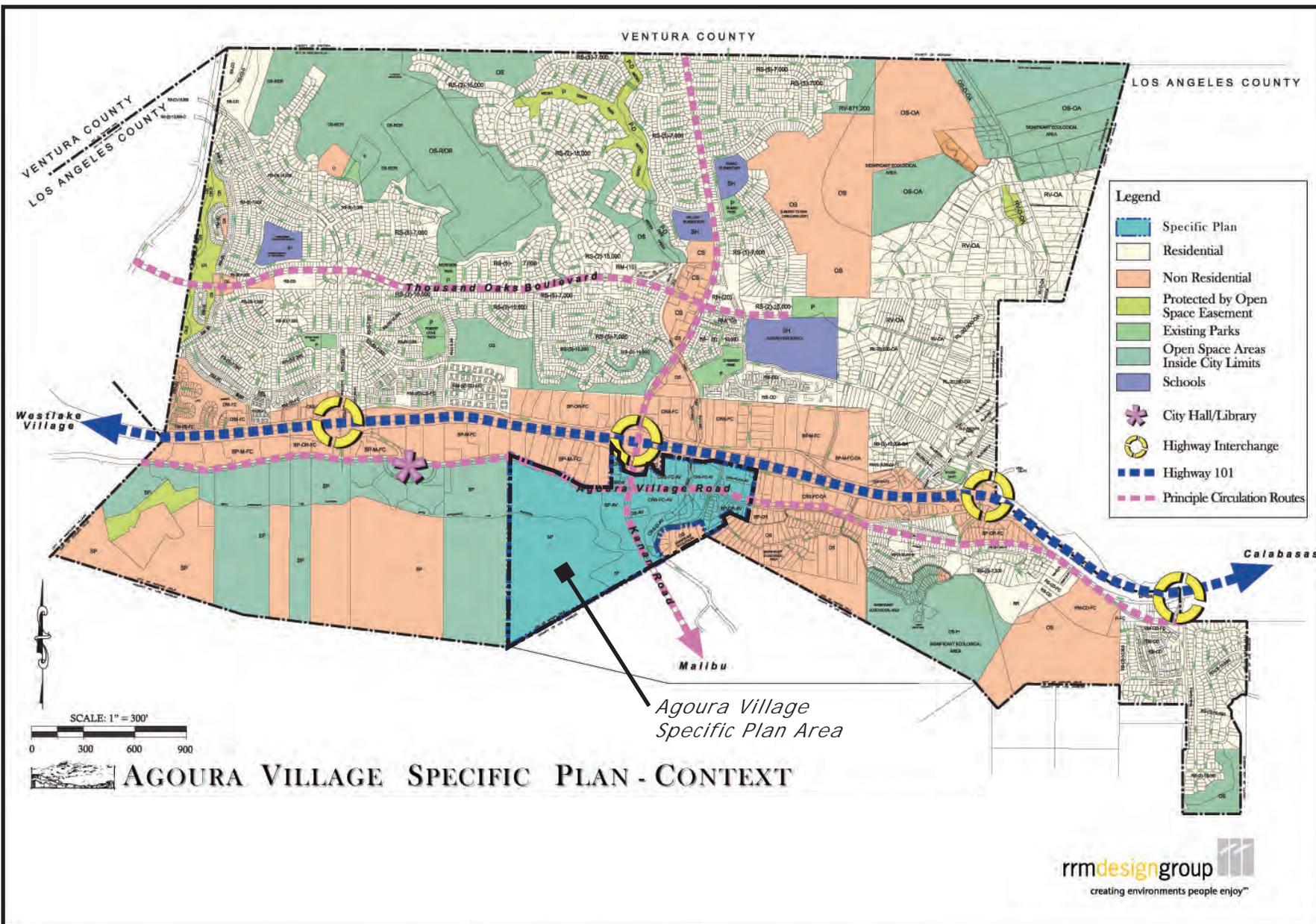
As a first step, RRM Design Group was contracted to create a Strategic Plan to provide a more in-depth analysis of the Agoura Village area. The Strategic Plan consisted of an Opportunities and Constraints Analysis Report, Architectural Design Guidelines, and an Urban Design Plan. These components, along with the public outreach that was conducted during that process, provide the foundation for the development of the Agoura Village Specific Plan.



Project location

The Agoura Village project area is located just south of the Ventura freeway (101) in the City of Agoura Hills. The area encompasses about 135 acres of developed and undeveloped area and is characterized by various existing commercial uses including retail, restaurant, office, and services. About 66 acres of the total acreage is vacant.

Figure 1.1 - Context Map



VENTURA COUNTY
LOS ANGELES COUNTY
LOS ANGELES COUNTY

- Legend**
- Specific Plan
 - Residential
 - Non Residential
 - Protected by Open Space Easement
 - Existing Parks
 - Open Space Areas Inside City Limits
 - Schools
 - City Hall/Library
 - Highway Interchange
 - Highway 101
 - Principle Circulation Routes

SCALE: 1" = 300'
0 300 600 900

AGOURA VILLAGE SPECIFIC PLAN - CONTEXT

Purpose

The Agoura Village Specific Plan is a comprehensive document that clearly identifies the vision for the planning area, articulates economic goals, provides regulations and guidelines for new development and redevelopment, traffic, regulatory, and physical issues. It establishes a framework for development within the area, with a logical system of circulation and parking, improvements to the streetscape, and a cohesive set of public improvements that will lead to the creation of a true pedestrian-friendly sense of place in the Village.

This Plan allows for more flexibility than what is presently allowed by City zoning regulations and requires more unique developments than those anticipated under existing rules and procedures. It establishes appropriate land uses, urban design concepts, architectural design guidelines, and sets into place regulations to implement the “vision.”

An Environmental Impact Report (EIR) has been prepared to evaluate environmental impacts of future development in the Specific Plan area and to mitigate the impacts of development.

Authority of the Specific Plan

A Specific Plan is a regulatory tool that local governments use to implement their General Plan and to guide development in a localized area. While the General Plan is the primary guide for growth and development in a community, the Specific Plan is able to focus on the unique characteristics of a specialized area by customizing the vision, land uses, and development standards for that particular location. This Specific Plan has been prepared pursuant to Section 65450 et seq. of the California Government Code.

The Specific Plan document customizes the standards and regulations found in the City Zoning Ordinance to help achieve the Agoura Village vision. In any instance where the Specific Plan conflicts with the requirements of the City of Agoura Hills Municipal Code, the Specific Plan

provisions will take precedence. Where the Specific Plan is silent on a topic, the Agoura Hills Zoning Ordinance requirements remain in force.

This Specific Plan is consistent with the Agoura Hills General Plan as it will be amended upon adoption of the Plan. It is the intent of the City Council that this Specific Plan will lead to the systematic implementation of the General Plan goals and policies that relate to, and control, development in the Agoura Village area of the city. Such goals and policies include, without limitation: Provides for a mix of land uses that meets the diverse needs of Agoura Hills residents, offers a variety of employment opportunities, and allows for capture of regional growth. In addition, the General Plan encourages the use of Specific Plans as a tool to implement general plan policies to provide for cohesive coordinated development with high profile areas of the community.

Document Organization and Content

The Specific Plan has been organized as follows:

Chapter 1 includes project background, including prior work efforts that lay the foundation for this plan. Also included in this chapter is a description of the vision for this area and the data gathering process.

Chapter 2 contains a detailed analysis of the existing conditions in the Village area, including a review of existing opportunities and constraints.

Chapter 3 focuses on mobility, illustrating the preferred circulation pattern throughout the Village. The plan places emphasis on pedestrian circulation, consolidated parking, and a connection to surrounding open spaces via a trail system.

Chapter 4 provides direction on the land use and development standards, depicting the overall layout of land uses within the Village, which are contained in seven different Zones. The Land Use chapter also includes a detailed description of development standards and allowable uses.

Chapter 5 Architectural Design Guidelines, and Chapter 6 Streetscape Beautification/Public Improvements, provide an outline of guidelines that define and illustrate the desired architectural character and proposed public right-of-way improvements for the Village.

The final three chapters in the plan (7 - Infrastructure and Public Services, 8 - Implementation, and 9 - Plan Administration) are intended to provide a foundation from which to guide the successful implementation of the Plan and ensure its objectives are integrated effectively with the goals of existing documents, including the City's General Plan and the Ladyface Mountain Specific Plan, which is adjacent to the Agoura Village Specific Plan area.

Public Involvement and Workshops

During the initial phase of the Strategic Plan, discussions with community stakeholders, City staff, and members of the City Council, Planning Commission, and other review bodies were held, revealing important information for the development of the Plan. These meetings were used to develop urban design and planning principles and to evaluate land use and circulation plans, streetscape improvements, and implementation actions for the Village area. The information gathered during this process was used to develop the Strategic Plan and ultimately the Specific Plan. The following meetings and workshops were conducted as part of the Strategic Plan effort:

Key Person Interviews

During the initial phase of the analysis process, a series of meetings over a two-day period were conducted. The City's consultants met with key stakeholders within the community, including various property owners, Planning Commissioners, City Council members, residents, and representatives from the Chamber of Commerce. Throughout the interview process, several common goals and concerns were revealed and were used

as the framework and guiding concepts of the Specific Plan. The issues have been divided up into related topics that include vision, land uses, circulation and traffic issues, natural amenities, and architecture. A summary of these interviews is provided below.

Vision

- Make it a place where residents and visitors alike can gather and meet.
- Generate regional appeal -something unique that creates a “draw” or attraction.
- Create an entertainment/family-oriented area.
- Encourage mixed-use projects.
- Recognize appropriate density (a key element for increasing a pedestrian oriented environment with gathering places).

Land Uses

- Consider economic competition such as the Calabasas Commons and the Westlake Promenade.
- Incorporate successful existing businesses.
- Create a retail “veneer” along Agoura Road or otherwise camouflage the mini-storage facilities.
- Provide a residential component to create a successful pedestrian oriented development.
- Allow opportunity for a potential hotel or inn.

Circulation and Traffic Issues

- Explore the option of on-street diagonal parking on Agoura Road.
- Consider impacts of beach traffic on Kanan Road.
- Explore traffic calming alternatives and provide pedestrian links across Agoura Road and strategically throughout the Specific Plan area.



Public Workshop Participation

Natural Amenities

- Recognize the significant value of creeks and provide for their rehabilitation.
- Encourage development of an equestrian center to link to the trail system.
- Acknowledge the area as a gateway to the Santa Monica Mountains.

Architecture

- Develop a consistent urban design thread of something special that also allows the architecture to vary.
- Recognize historic buildings and significant places.

Agoura Village Task Force

To help guide the development of a plan for the Village, the Agoura Village Task Force was created. On June 27, 2002, the Agoura Village Task Force (comprised of representatives from the City Council, Planning Commission, property owners, and businesses) met for the first time. City staff and the consultants met with the Task Force multiple times throughout the planning process to provide advice, feedback, and direction on various aspects of the Strategic Plan.

Staff Meetings and Interviews

Regular meetings with City staff were held throughout the project to discuss a range of possible design concepts and to identify relevant capital improvement projects and other planned community development or redevelopment activities that affect the Village area. Staff and consultants also met with Task Force members, property owners, and interested developers in a series of meetings to get realistic feedback about potential development. The discussions and identification of issues relevant to this project include traffic safety and circulation issues, funding limitations, maintenance issues, urban design character, and public participation.

Joint City Council/Planning Commission Meetings and Workshops

On July 31, 2002, a joint meeting with the Planning Commission and City Council was conducted. The results of an existing physical constraints/opportunities analysis, market feasibility study, traffic analysis, and regulatory analysis were presented at the workshop, and the planning team began a discussion regarding key policy matters.

Following the July 31st meeting, the planning team and staff conducted additional analysis and refinements to the Strategic Plan. These included the development of the draft Agoura Village Master Plan illustrative, the preparation of a land use intensity and phasing plan, the development of a traffic circulation plan, and the preparation of draft policy and regulatory amendments that would be necessary to implement the Agoura Village Strategic Plan and, ultimately, the Specific Plan.

At the January 29, 2003, Joint City Council/Planning Commission Agoura Village Workshop, staff and the City's consultants provided an update on the work completed to date. The discussion following the update led to direction on certain key policy matters that were then used to develop this Plan.

Public Open House

On July 16, 2002, a public open house was conducted where over 60 people were in attendance. An overview of the project was provided and the conceptual Urban Design Plan was presented. The intent of this public workshop was to dialog with residents, business owners, and property owners and outline additional opportunities to participate in the development of the Strategic Plan.

Specific Plan Vision and Planning Principles

Vision

The vision for Agoura Village is to create a welcoming pedestrian friendly atmosphere that captures the character of Agoura Hills. The Village will become a destination and not just a pass through area for highway users and visitors to the area. It will be shaped into an identifiable and inviting place with an intimate streetscape lined with unique storefronts and will become a comfortable place to gather, shop, and stroll.

To achieve this vision, the Village area will begin a transition from its current state toward a unique balance of land uses over time, including a mix of pedestrian activities along Agoura Road. This will improve the area for both visitors and residents, creating a unique destination in the City of Agoura Hills. In the Plan, there will need to be a balance of new residential units to provide support for the new retail development, and Agoura Road will undergo a transformation to foster new development within the area.

As Agoura Village develops, Agoura Road will be transformed into a pedestrian-oriented street with retail shops, restaurants, theatres, and entertainment uses. Secondary to the retail and entertainment uses, limited new residential development will provide support for the new retail development and help create a daytime and evening environment. While retail, restaurant, customer-friendly commercial service, and entertainment uses should remain the focus of the Village, uses such as office and residential are appropriate as secondary uses to help sustain a village-like environment. Residential uses only will be allowed as part of an integrated mixed-use project concept. An important goal of the Plan is to limit the total amount of residential development in the Village to create a vibrant village and pedestrian-oriented mixed-use environment. To ensure the mix of uses is carried out in the Village, a significant amount of commercial development

“Village Concept Plan” Vision

The development of Agoura Village will transform the Agoura Road corridor into a pedestrian-oriented center with retail shops, restaurants, theatres, and entertainment uses. Agoura Village will offer a unique shopping, dining and entertainment experience where visitors can stroll within a pedestrian-oriented village. The natural setting offered by the surrounding oak covered hillsides, creeks, and views of ladyface Mountain will enhance the shopping experience and will create a “Uniquely Agoura” sense of place.



AGOURA HILLS



Traffic Roundabout with Plaza & Building Frontage with Pedestrian Crossings

Roadside Drive Beautification

Agoura Village Center
MIXED USE:
Restaurants, Retail, Specialty Shops with Office & Residential Above
Plazas
Pedestrian Oriented Design
Village Style Architecture
Diagonal Parking

Mann Theater Complex
Retail & Entertainment Center

Whizen Center
Retail & Entertainment Center

Cornerstone at Agoura Village

Restore Chesebro Creek

Agoura Village Shopping Center/Padris

Restore Medea Creek

Community Center

Hiking & Equestrian Center

Kanan Road Beautification & City Entrance Element

Preserve or Restore Creek Corridors, Oak Groves, & Areas of Significant Native Vegetation

Additional Mixed-Use Development

Agoura Village
MIXED USE:
Retail, Office, Hotel, Specialty Shops & Residential Above

Offices

Trees Screen Storage Along Agoura Road

Chuy's & Islands

Figure 1.2 Agoura Village Vision

is scheduled to be developed concurrent with residential development.

The Plan also requires the establishment of selected public uses, such as a public plaza, a center to accommodate community events, child care uses to accommodate the needs of workers, specialty grocery stores to attract residents from both inside and outside the Village, and a hotel or inn to serve visitors.

Development in Agoura Village will be designed to respect and fit into the natural surroundings by protecting view corridors, retaining oak trees, preserving rock outcroppings, restoring creek and riparian habitat, and planting native landscaping. Shared parking will be provided behind the building street frontage so that one can simply park once and walk to the various businesses within the Village. The central community plaza will include landscaping supportive of the natural setting, plaza furniture, and public art to enhance the pedestrian experience. Agoura Road will undergo a transformation with streetscape upgrades to facilitate new development within the area.

Planning Principles

As part of the visioning process a number of planning principles and design concepts were considered and implemented. They include mixed-use development, urban and environmental design, pedestrian orientation, circulation and parking, and natural resource protection.

Mixed-use Development

A cornerstone of the Agoura Village Specific Plan is achieving diversity and character through a mixed-use Village environment. Mixed land uses are critical to achieving great places in which people can live, work, and play. A mixing of uses helps to add variety and vitality to commercial centers, neighborhoods, and streets. It also makes a place more attractive and interesting, contributing to a balance of different types of activities and creates a place that does not become deserted after work hours.



Vertical mixing of uses can create a dynamic place of activity and add a 24-hour life to the Village

Both vertical and horizontal mix of uses will help achieve the vision for a dynamic center. A horizontally mixed-use project will incorporate stand alone residential uses next to stand alone commercial, office, and entertainment uses on a given site, while a vertical mix of uses will contain residential and/or office located above retail or commercial within the same building.

The following principles were developed to help define the vision for a mixed-use pedestrian-oriented village.

- Establish the project area as a “destination” accommodating a range of uses that serve Agoura Hills’ residents and visitors.
- Design the Village to reflect the unique character of Agoura Hills and quality of life; do not replicate similar pedestrian-oriented centers in the region.
- Promote a diverse mix of uses that maintains activity throughout the entire week, not just weekend nights.
- Plan for entertainment and community-serving retail and related uses, such as specialty markets and bookstores.
- Provide new public and quasi-public uses, such as a new community center, playhouse/theater, or other family-oriented uses.
- Consider development concepts consisting of themed retail, local office support, hotel visitor base, recreation (family-oriented), and mixed-use housing.
- Provide housing above ground level commercial uses.
- Emphasize the development of uses that enhance/attract pedestrian activity.
- Incorporate a public plaza that serves as the center of community events and celebrations (e.g., farmers markets, picnics).
- Plan for limited medium-density (attached townhomes and apartments) stand-alone residential use with the intent of supporting the primary commercial and retail uses.

- Prohibit certain land uses that erode the desired pedestrian village concept, such as mini-storage, building supplies, drive-thru restaurants, auto repair, etc. Allow existing non-conforming uses to conform subject to City ordinance.
- Focus a greater amount of residential development in the core of the Village than in the peripheral areas to support a mixed-use village environment.
- Ensure that residential development remains secondary to commercial retail development on a site.
- Design tenant spaces to allow for an easy transfer of uses and tenants in order to establish an opportunity for businesses to turnover and transition while minimizing the impact to a vital downtown.

Site Layout

One of the keys to the success of the Village will be to focus on the physical form of the built environment with the end goal of producing a specific type of ‘place’. The creation of streets and blocks, where a building is placed on the lot, the general shape and size of the building, and where parking areas and yards and courtyards are located is just as important as the architecture of the buildings. While creating streets and blocks throughout the Village is key, it is also important that Agoura Road develops as a “Main Street” for the area. Therefore, buildings located adjacent to Agoura Road should be oriented towards the street.

The goal is to create a walkable, vibrant place. Parcels in the area should be separated into blocks that in turn create a walkable public realm of varying streetscapes that serve human-scaled lots and human-scaled buildings. Proactively using a system of blocks and streets that clearly define where buildings go and where streetscapes go will create truly walkable and human-scaled places, particularly at the village-scale. An extensive, connected network of public spaces is also important.

Pedestrian-scale design balances automobile circulation and focuses on making environments that are comfortable and pleasant. It is important that parking be placed out of sight as much as possible and that no large parking lots dominate the visual realm. Subterranean parking for both commercial and residential development is highly encouraged so that the Village will not be visually dominated by surface parking lots and the associated imagery. An alternate is at-grade parking garages that are 'lined' with shop or residential space to conceal the garages from public view.

Urban and Environmental Design

Urban and environmental design techniques can be used to create a distinctive image for the Village area. Urban design techniques are incorporated into the Specific Plan Area to tell visitors they have entered a unique and defined place. This "branding" should include creation of a graphic, icon, or logo; new gateway monuments and signage; decorative paving features; and landscaping. These improvements will help to increase the quality of the pedestrian environment and to help slow down automobile traffic. In the Village, the proposed roundabout will function as a primary gateway to the core.

- Create a unified streetscape along Agoura Road to help bring together the visual and thematic experience of the Village.
- Incorporate new decorative street furnishings, banners, and light standards that reflect the desired character of the Village into the streetscape design.
- Strongly encourage two-story development and massing along Agoura Road and within the Village to bring a comfortable scale to the street and to reduce the impact of the large street right-of-way.
- Reduce building heights and terrace buildings located along the hillsides to complement the natural terrain.

- Require additional detailing and architectural features on building facades, such as awnings or trellises, to create a human scale and a pedestrian-friendly environment.
- Provide a community plaza in the heart of the Village to provide an opportunity for community gathering.
- Screen undesirable and non-conforming uses with landscaping or retail-oriented façades.

Pedestrian Orientation

Creation of new improvements and enhancement of existing pedestrian spaces within the Plan area will encourage pedestrian orientation and movement throughout the area. This includes enhancements to the street environment with new bulbouts, crosswalk paving, and sidewalks.

- Provide “bulbouts” (sidewalk extensions) at key locations to help minimize pedestrian crossing time and increase pedestrian safety.
- Embellish crosswalks with accent paving to add to the visual appeal of the area as well as direct pedestrians and autos where people should cross Agoura Road.
- Create opportunities for enlarged sidewalks so people feel safe and comfortable walking throughout the Village area.
- Site storefronts close to the street to better define the street edge. A continuous storefront experience and attractive walkways will maximize the quality of the pedestrian environment.
- Promote some variation in front setbacks to allow for outdoor dining and other sidewalk activities, which will bring vitality and interest to the street edge.
- Implement a clear wayfinding system to visually tie the area together and to direct patrons to public parking, stores, public spaces.

Circulation and Parking

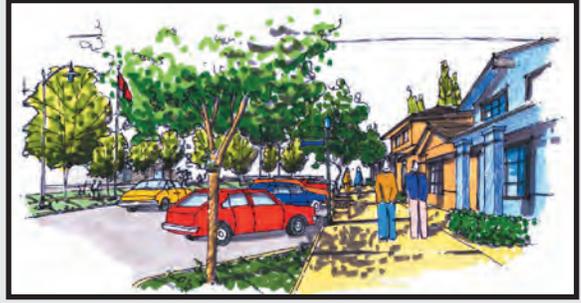
Circulation improvements for pedestrian, bicycle, and vehicle needs are an important objective for realizing the vision for the Village. The following principles have been incorporated into the Specific Plan.

- Reduce Agoura Road in the heart of the Village (between Kanan and Cornell) to two lanes with a landscaped median in the center and diagonal parking on either side of the street.
- Construct roundabout at Kanan Road and Agoura Road to provide a better traffic flow, calm traffic, and provide visual interest to the area.
- Provide diagonal parking on Agoura Road and Cornell Road to accommodate short-term parking needs for the area and reduce crossing distances for pedestrians.
- Plan for new off-street public parking areas to allow for longer term parking for visitors, residents, and people who work in the area.
- Create a parking district with the development of shared parking facilities, on-street parking, and opportunities to reduce parking that can support multiple businesses.
- Consider the development of an Agoura Village shuttle and/or connection with existing City transit service that conveniently links the area’s businesses and sites.
- Create paseos in new development linking internal parking and residential areas together and to public streets.
- Establish a trail system along Medea Creek and Lindero Canyon Creek connecting Agoura Village to the Zuma Ridge Trail portion along Agoura Road and to residential areas north of Highway 101.
- Plan for broad sidewalks on all primary streets that are attractive and well lit.

Natural Resource Preservation

Preservation and restoration of natural resources such as creeks, native vegetation, trees, and unique topographical features is an important component of the Village vision. Natural resources should be integrated into the planning considerations for proposed developments in an effort to reduce the potential impacts on these resources.

- Incorporate passive recreation and open space uses that capitalize on the area's natural setting and proximity to surrounding open space.
- Minimize grading and protect the natural surroundings. Graded slopes should be rounded and contoured to blend with the existing terrain. Significant natural vegetation should be retained and incorporated into the project whenever possible.
- Retain views of the natural surroundings.
- Promote the preservation of open space.
- Incorporate new oak trees and other native species into the landscaping of future development.
- Promote the development of an equestrian center or similar recreational use.
- Expand upon existing hiking and equestrian trail system.
- Promote the restoration of Medea, Chesebro, and Lindero Canyon creeks.



Pedestrian-oriented buildings and sidewalks welcome people to walk within the Village



Roundabouts provide a better traffic flow, calm traffic, and provide a visual interest to the area

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CHAPTER 2: EXISTING CONDITIONS

Introduction

Currently, the Specific Plan area consists of a wide mix of uses including several small restaurants, a movie theater complex, self-storage facilities, building materials, a gas station, a lumberyard, and several home improvement businesses (refer to Figure 2-1). There are no sidewalks along Agoura Road, and the uses are neither visually nor physically linked. Aside from the restaurants and the movie theater, there is presently little reason nor places to walk to get from one location to another. A shift in some of the existing uses, development of new commercial/ retail projects, introduction of residential uses, and the implementation of sidewalks and street trees will all be important catalysts in the creation of a “village” environment.

The following summary of existing conditions contains important findings that help to shape the Plan:

- Existing physical conditions
- Circulation and traffic conditions
- Natural resources
- Economic market conditions

A comprehensive Opportunities and Constraints Report (Refer to the Appendix) was prepared as a part of the Plan analysis.



Existing right-of-way on north side of Agoura Road



Existing pedestrian and equestrian connection at Mann Theater complex

Existing Physical Conditions

The following summary is of existing physical conditions within the Specific Plan area.

Existing Development

Many parcels within the study area are improved with existing development. These include building materials and supplies, self-storage, restaurants, gas stations, and others (Refer to Figure 2.2 - Existing Conditions Analysis). Many of the existing uses are incongruent with the goal of creating a pedestrian-oriented mixed-use destination in Agoura Hills. Additionally, many of the existing uses have been located in the project area for a long time, have contributed to the City's economic condition, and have relatively high real estate values. These conditions will make it difficult to redevelop or relocate existing uses in the short to mid-term. Therefore, a long range solution must be developed which supports the viability of these businesses while creating opportunity to beautify, revitalize, and redevelop them into uses that are consistent with the area vision. In the short term, uses and existing buildings such as the self storage facility, the Los Angeles County flood control area and Las Virgenes Municipal Water District building may need to be screened or ultimately relocated to create a Village atmosphere.



Existing landscape buffer between Agoura Road and self storage facility

Development Opportunities

About 29 parcels, consisting of 66 total acres, are vacant within the project area. The two largest parcels are on the south side of Agoura Road flanking Kanan Road and are at the center of the Village area. The future development of these parcels will have a great impact on the vision and future of the Village.

Gateways

Kanan Road is the primary intersecting thoroughfare within the Specific Plan area. The majority of residents that live within Agoura Hills reside on the north side of Highway 101 and access the Village via Kanan Road. In addition, motorists from surrounding communities arrive either from the Highway 101 off-ramp at Kanan Road to the north or through the Santa Monica Mountains connecting to Malibu to the south.

Potential gateway locations are provided where the Specific Plan boundary intersects Agoura Road and Kanan Road. Refer to Figure 2.1 (Opportunity and Constraints Analysis) for proposed gateway locations and Chapter 6 (Streetscape Beautification and Public Improvements) for a discussion of gateway treatments.

Topography

The southern boundary of the Specific Plan area is adjacent to the base of Ladyface Mountain and designated as open space. Hillsides of the Santa Monica Mountains have high visibility, natural resources, and geologic conditions that limit potential development within these areas. The vacant parcel at the southwest corner of Kanan Road and Agoura Road is particularly challenged with topographical and geological constraints.

Figure 2-1 - Opportunities and Constraints Analysis

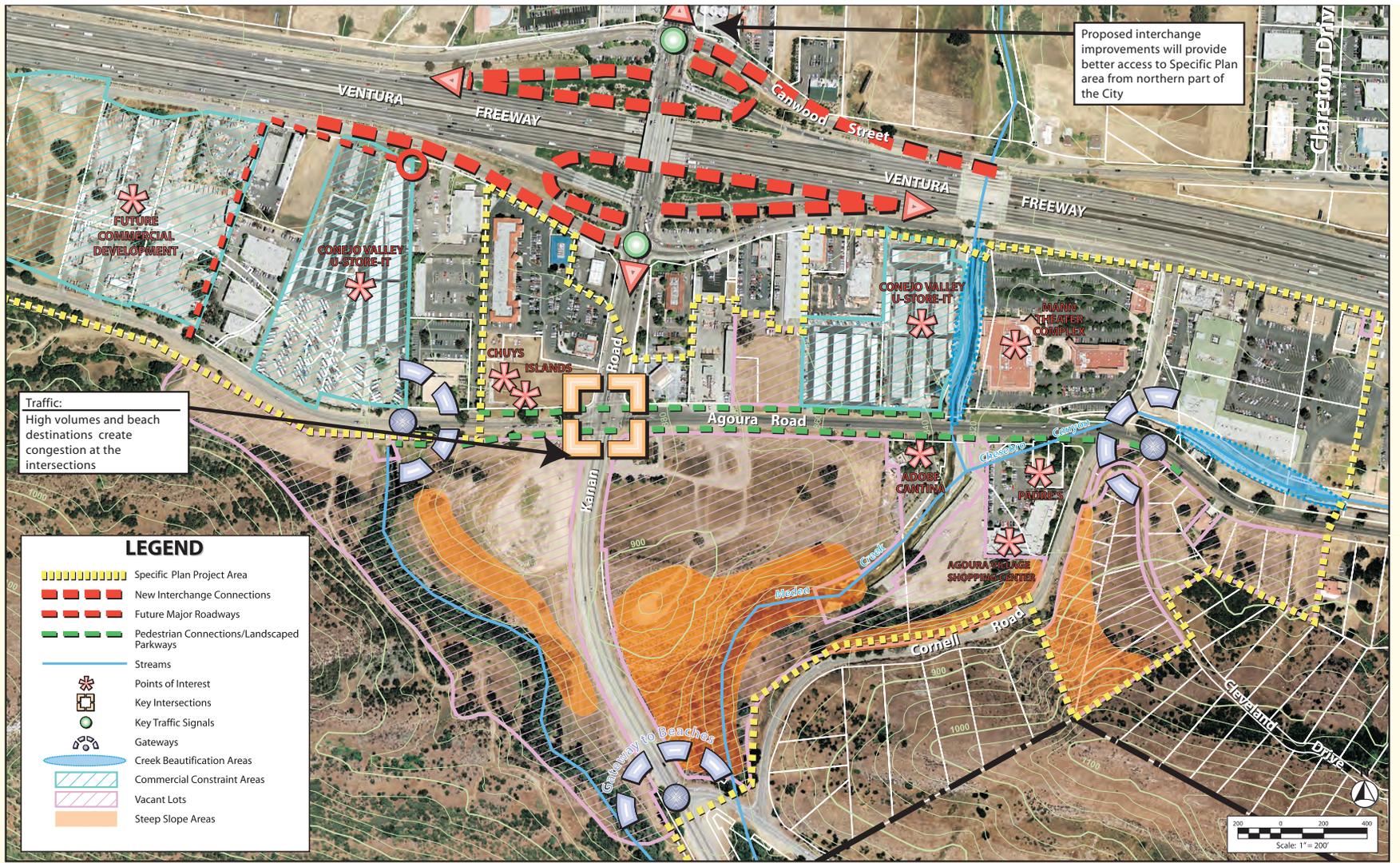
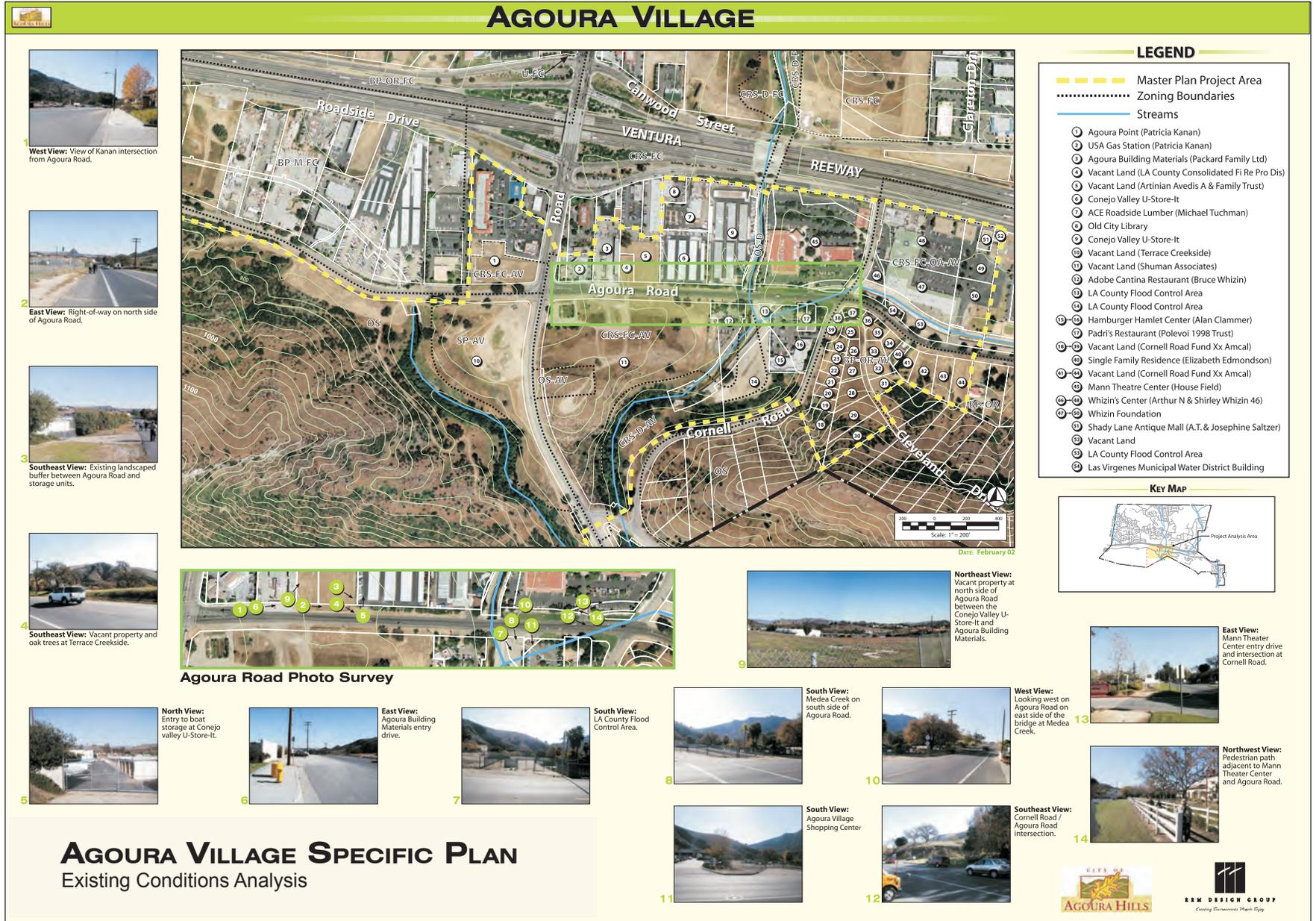


Figure 2-2 - Existing Conditions Analysis



Agoura Road has a moderate slope and reaches a plateau just east of Kanan Road, which then begins to drop in both directions. Mid-block crossings will have to be strategically located due to low visibility on Agoura Road. Topographical changes also challenge pedestrian connections along Agoura Road between the corner of Cornell Road and Kanan Road.

Natural Resources

The area is located in an attractive setting, with oak trees and rolling hills, that has the potential to create a pleasant pedestrian-oriented Village with outdoor areas and dining to take advantage of the natural visual resources. There are three creeks that run through the area. Currently, Medea Creek and Chesebro Creek have been converted into concrete drainage channels (covered in some places) that run along Agoura Road just south of the Whizen Center and the Mann Theater Complex/Agoura Cinema Center. These channels should be restored to a more natural state where new development could be designed to take advantage of the resources, as well as becoming a feature to connect the Whizen Center and the Agoura Cinema Center.

Lindero Creek in the western portion of the project area is in a natural condition and should be considered an asset to protect and use as a resource in site planning.

Important natural habitat and views into the Santa Maria Mountains should be preserved and protected through the policies of the Plan.



Medea Creek on south side of Agoura Road



Vacant lot between self storage facility and building materials yard



Vacant property and oak trees at Agoura Road and Kanan Road

Summary of Existing / Physical Constraints and Opportunities

- Approximately 29 parcels consisting of 66 acres are vacant within the Specific Plan area.
- A wide mix of uses including small restaurants, a movie theater complex, self-storage facilities, a gas station, and a lumberyard and home improvement businesses are currently in the Specific Plan area.
- There are currently no sidewalks along Agoura Road and the uses are not visually or physically linked.
- The self storage facilities and other incompatible uses along Agoura Road and Medea Creek and should be screened.
- Potential Village gateways are located at Kanan and Roadside Drive, Kanan Road and Cornell Road, Cornell Road and Agoura Road, and just west of the western boundary area near the self-storage facility.
- The southern boundary of the Specific Plan area is adjacent to Ladyface Mountain and areas designated as open space.
- Steep slopes and volcanic rock limit the developable area of the vacant parcels adjacent to Kanan Road on the south side of Agoura Road.
- Three creeks run through the project area.

Market Conditions

A Market Feasibility Analysis was conducted as part of the existing conditions analysis. The purpose of the Analysis was to identify new development opportunities that will create a retail and community focal point in the City while contributing to its economic well-being. This will involve integrating existing development in the Specific Plan area with new mixed-use development. The locational advantages of the area, including its prime access off Highway 101 and the well-traveled arterial Kanan Road, give the Agoura Village area the potential to become a vibrant town center. However, the tenant mix and configuration of the existing retail centers in the project area need to be addressed in order for the City to establish a competitive position with centers in nearby communities.

Economic Strengths and Weaknesses

An understanding of the economic strengths and weaknesses is important for feasible Specific Plan land uses and implementation strategies.

Strengths

- The plan area is situated in a prime location. Agoura Road is a main artery through the City and has multiple points of freeway access, making it a local alternative to the freeway. Agoura Road could provide the activity required to support a variety of retail activities. In addition, traffic flows north and south on Kanan Road to the coastal communities and beaches.
- The current Agoura Village retail centers provide an eclectic mix of retail, including antique shops, restaurants, entertainment, and retail shops. The restaurants include popular eating establishments such as Chuy's and Islands, as well as the Adobe Cantina and Padri's, which draws patrons from as far away as Malibu.
- Overall, the plan area generates about \$2.0 million in total sales tax revenues, or about 80.0 percent of the City's total sales tax revenues. This includes both retail and non-retail taxable sales.

- The drainage channel that runs across Agoura Road in the eastern portion of the project area could be redesigned to become an attractive landscaped promenade for outdoor dining and lounging as well as a feature to connect the various retail opportunities.
- The area is located in an attractive setting, with oak trees and rolling hills, that has the potential to create a pedestrian-oriented Village with outdoor areas and dining.
- The existing office and hotel base in the area is a source of demand for retail and entertainment activities. Currently, there is new office development occurring adjacent to the plan area, and the 94-room Hampton Inn was recently constructed west of the project area.
- Within a 1.5 mile radius of the Agoura Village plan area, about 48.1 percent of the total households have members who are less than age 18. Approximately 38.2 percent of the households south of the freeway within this 1.5 mile radius have households with members less than age 18. This indicates that there is potential for family-oriented retail, recreation, and entertainment uses within the project area.

Weaknesses

- The potential for retail in certain categories is limited due to the large supply in the competitive centers. This includes food stores, apparel, home furnishings, and general merchandise.
- There is a limited population base directly around the Agoura Village area. Most of the residential population of Agoura Hills is north of the freeway, creating a psychological barrier that must be overcome if shoppers are to be attracted to the area south of the freeway.
- The existing retail centers lack connectedness between each other that would be necessary in a town center to provide the full range of pedestrian-oriented activities and a sense of place.

- The existing vacant sites pose physical constraints, such as the slope of the terrain and soil conditions, that will have to be addressed in order to construct new development.
- The costs for providing new infrastructure along Agoura Road, improvements required for road widening, and enhancements to Kanan Road will be substantial and may require additional sources of financing.
- The current configurations of the Whizen Center and the Agoura Cinema Center at the eastern segment of the project area face Roadside Drive and Highway 101 as their primary retail window rather than Agoura Road. Refocusing development along both sides of Agoura Road has the potential to create more of a community environment.

Summary of Market Opportunities

The Market Feasibility Analysis (under separate cover) addresses the potential for a range of development opportunities. Potential development concepts for the Agoura Village project area include themed retail as the primary concept, as well as others:

- Themed retail: Focuses on a “lifestyle center” concept that includes retail related to Leisure Time Activities, Visual Arts, Culinary Arts and Decorative Home Improvements.
- Local office support: A mix of office-oriented retail and services such as office-supply and stationery stores, personal services, and child care facilities could serve the concentration of office development in the area.
- Hotel visitor base: A mix of services, retail, and restaurants could draw on the nearby Renaissance Hotel and Hampton Inn visitor base, and there may be potential for additional hotel development.
- Recreation/family-oriented: Focuses on uses that combine recreational activities with retail and provide a unique attraction. This type of activity could draw patrons for the local hotels and serve as a local meeting place with its banquet facilities.
- Mixed-use housing: Opportunities exist for mixed-use in the project area. Housing market trends indicate that housing demand is strong and supply is dwindling. Therefore, it is likely that housing in the project area would be absorbed rapidly in today’s market.

Circulation

Existing Roadways

The Specific Plan area includes portions of Agoura Road, Kanan Road, Cornell Road, and Roadside Drive and the following key intersections: Agoura Road/Kanan Road and Agoura Road/Cornell Road.

- Agoura Road is a two-lane facility with wide shoulders striped with white edgelines. There are no curbs, gutters, or sidewalks along Agoura Road, except for the section near Cornell Road where there are curbs, but no sidewalks.
- Kanan Road is a four-lane road between Agoura and Highway 101 that provides access to the U.S. 101 interchange. Kanan Road transitions to a two-lane road south of Agoura Hills with a center left turn lane.
- Cornell Road is a two-lane rural road that provides access between Kanan Road and Agoura Road.
- The intersection of Kanan Road/Agoura Road is controlled by a traffic signal.

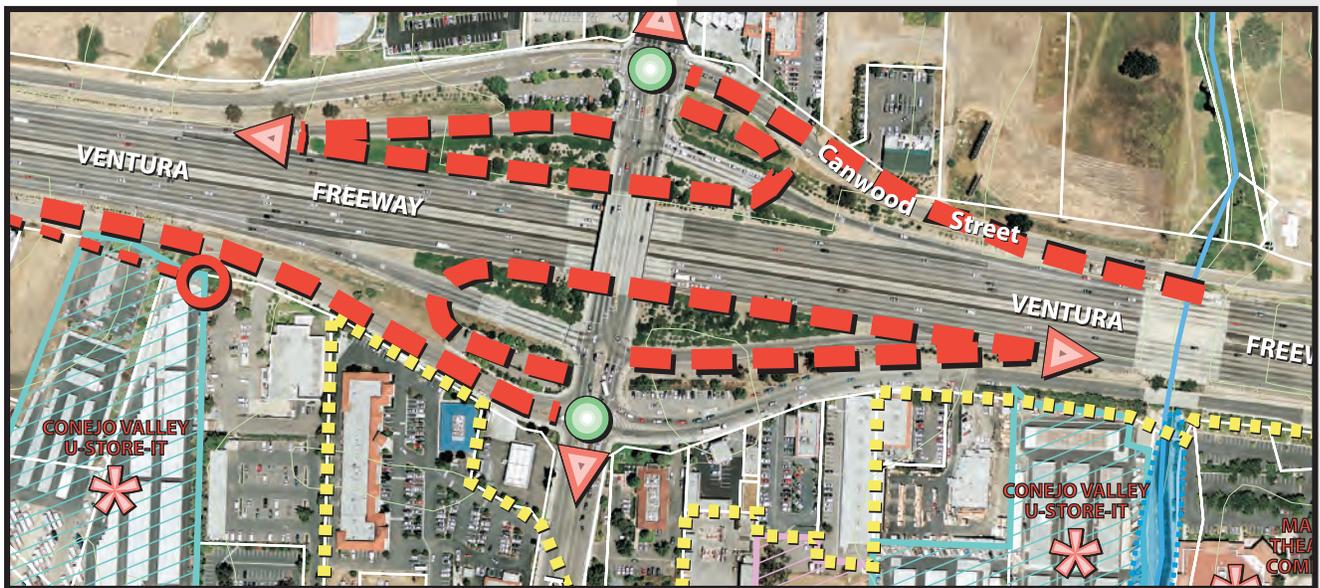


Figure 2-3 - Highway 101 Interchange at Kanan Road

- The intersection of Agoura Road/Cornell Road is controlled by stop signs on all approaches.
- The intersection of Roadside Drive and Cornell Road is a three-way segment controlled by stop signs on all approaches.

Intersection improvements were completed at Kanan Road/Agoura Road in 2004. These improvements consisted of the addition of protected left-turn phasing for northbound and southbound traffic as well as a widened approach on eastbound Agoura Road.

Although not part of the Specific Plan area, the U.S. 101 interchange at Kanan Road is slated to be improved. These plans include relocation of the southbound off-ramp to align with Roadside Drive, an added southbound on-loop from southbound Kanan Road, and relocation of the northbound U.S. 101 ramps to Canwood Street.

Existing Bicycle and Pedestrian Facilities and Transit Service

Currently, there are no striped bicycle lanes and few pedestrian paths along Agoura Road in the Specific Plan area and only minimal pedestrian facilities along other streets. The signal at Agoura Road/Kanan Road includes pedestrian crossing controls on all four legs of the intersection. There is a mid-block pedestrian/equestrian crossing adjacent to Medea Creek west of Cornell Road. The crosswalk serves a short off-road pedestrian/equestrian path between Cornell Road and the crossing location.

Although bike paths are not striped on Agoura Road, the existing shoulders with white edgeline striping are wide enough to accommodate bicycle travel and Agoura Road serves as a regional bike route.

The following transit facilities are available in the City of Agoura Hills:

- Metropolitan Transit Authority Route 161 provides access between Thousand Oaks and Canoga Park. In the Plan area, the bus stops at the Kanan Road interchange.
- The City of Agoura Hills operates a Dial-a-Ride service that provides transportation anywhere within the City limits.

Traffic Circulation Summary

A traffic study conducted during the Strategic Plan process revealed the following opportunities and constraints for the area:

- The Agoura Road/Kanan Road intersection is currently operating at an acceptable level of service. Future improvements, however, will likely be needed.
- Existing pedestrian crossing opportunities exist at the Agoura Road/Kanan Road intersection and along the east side of the flood control channel just west of Cornell Road.
- Adequate sidewalks and/or pedestrian paths should be provided on all streets throughout the project area.
- On-street parking, either parallel or diagonal, should be considered in select locations.
- Agoura Road serves as a regional bike route (Class II).
- An enhanced crossing should be provided near the existing mid-block crossing as this location provides the best sight distance for approaching motorists.

CHAPTER 3: MOBILITY

Introduction

This chapter of the Specific Plan discusses the role of mobility to support the vision and goals for the Specific Plan area. This chapter presents improvement opportunities for Agoura Road, Kanan Road, and other streets in the Specific Plan area, as well as a description of the proposed roundabout, traffic calming concepts, bicycle facilities, equestrian trails, parking, and transit opportunities. More detail on the street furnishings and street signs is outlined in Chapter 6 (Streetscape Beautification and Public Improvements).

The Specific Plan strives to create a pedestrian-friendly destination. The Mobility chapter is intended to create a village environment with an active, engaged, human-oriented streetscape where the car is not viewed as the only mode of travel for the people who live, work, shop, and play here.



Roundabouts provide a better traffic flow, calm traffic, and provide a visual interest to the area.



Diagonal parking minimizes right-of-way width and enhances the pedestrian environment

Traffic Calming Elements

As the Village develops and new uses occur along Agoura Road, it is important to create a street environment where pedestrian circulation takes higher priority. Traffic calming design elements aim to balance the needs to effectively moderate vehicle speeds and to improve the pedestrian environment. Traffic calming measures include curb extensions, accent paving, street trees, medians, and other techniques.

Curb Extensions/Bulbouts

Curb extensions, often known as “bulbouts,” shorten pedestrian crossing distances and improve their visibility to motorists. They also widen the sidewalk where space is often needed for ramps, signal poles, and street furniture.

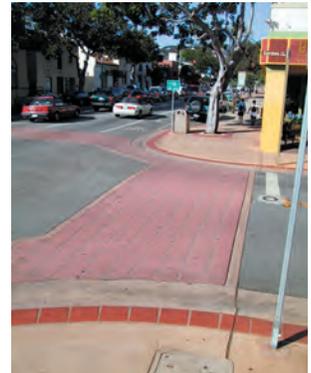
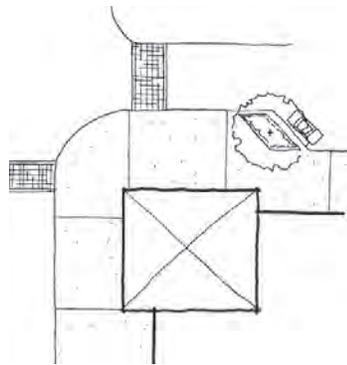
- Where appropriate, curb extensions shall be placed along Agoura Road at crosswalks, between diagonal parking areas, and at the mid-block crosswalks proposed at Medea Creek and the public plaza generally located as shown in the vision plan.



Mid-block crossing

Accent Paving

Accent paving such as unit pavers or colored concrete can be used on crosswalks to accentuate pedestrian crossings. The change in texture gives motorists a visual and audible heightened awareness which in turn helps to slow traffic. Refer to Chapter 6 (Streetscape Beautification and Public Improvements) for additional information on accent paving and pedestrian crossings.



Example of “bulb outs” and accent paving at crosswalks

Street Trees

Street trees offer an aesthetic alternative to the wide-open speedway feeling of a treeless arterial. Street trees planted at the sidewalk edge and in medians have a traffic calming effect as they create the perception of a visually enclosed, narrower street.

- Additional street trees shall be planted on Roadside Drive, Agoura Road, Kanan Road, and Cornell Road.

Medians and Refuge Islands

Medians can help improve the overall appearance of streets and can help slow traffic. In terms of safety, raised medians reduce conflicts between pedestrian and vehicles because they allow pedestrians to cross only one direction of traffic at a time. Medians can also be used to create pedestrian “refuge islands.” Refuge islands are extensions of the median that create a protected crosswalk in the middle of the street.

- A planted median and pedestrian refuge island shall be incorporated into the Agoura Road streetscape improvement.

On-Street Diagonal Parking

While the addition of on-street parking, either parallel or diagonal, will create increased delay along the corridor, its inclusion should be based not on the capacity of the street but the need to create a pedestrian friendly environment. Parking not only provides opportunities for easy access to the street activity, but also creates a buffer between the sidewalk and roadway.

- On-street diagonal parking may be placed along Agoura Road east of Kanan Road.
- Diagonal parking may be placed on the east side of Cornell Road.
- Parallel parking may be placed on the west side of Cornell Road north of Agoura Road and along the south side of Roadside Drive.

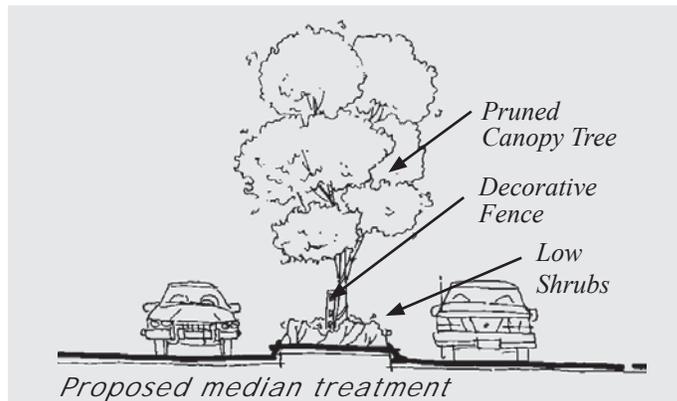
Narrowed Travel lanes

Narrowing travel lanes encourages slower vehicle speeds and reduce the pedestrian crossing distances. Drivers have been found to travel more slowly on streets with narrower lane widths. The effect is largely psychological. Narrower travel lanes and street widths require more attention from drivers. They also have the benefit of reducing pedestrian crossing distances (which is also a safety benefit) and freeing up space for other uses such as parking, bike lanes, medians, and widened sidewalks.

- The street cross sections provided in the following section of this chapter provide for minimum travel lanes widths while still meeting traffic engineering needs.

Roundabouts

Roundabouts have proven to be an effective way to improve the flow of traffic, reduce vehicle speeds, decrease the number of accidents, and increase pedestrian safety at an intersection



Roundabouts and colored/textured paving create a special intersection treatment



Example of a two lane roundabout

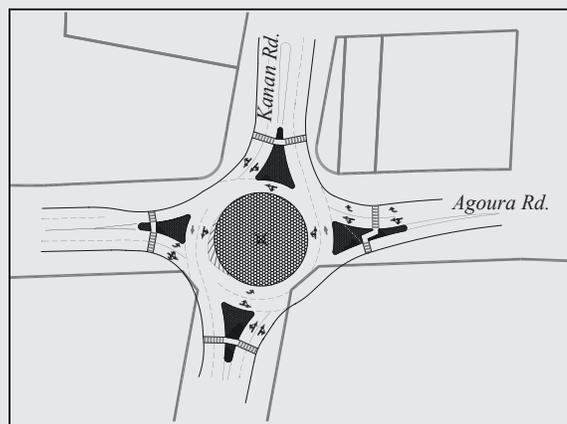


Figure 3.1 - Concept diagram of Agoura Road and Kanan Road roundabout, subject to final engineering approval

while providing a unique focal point to the Plan area. The following excerpt was taken from the US Department of Transportation–Roundabouts: An Informational Guide:

- “Roundabouts have fewer conflict points in comparison to conventional intersections. The potential for hazardous conflicts, such as right angle and left turn head-on crashes is eliminated with roundabout use.
- Low absolute speeds associated with roundabouts allow drivers more time to react to potential conflicts, also helping to improve the safety performance of roundabouts.
- Since most road users travel at similar speeds through roundabouts, i.e., have low relative speeds, crash severity can be reduced compared to some traditionally controlled intersections.
- Pedestrians need only cross one direction of traffic at a time at each approach as they traverse roundabouts, as compared with unsignalized intersections. The conflict locations between vehicles and pedestrians are generally not affected by the presence of a roundabout, although conflicting vehicles come from a more defined path at roundabouts (and thus pedestrians have fewer places to check for conflicting vehicles). In addition, the speeds of motorists entering and exiting a roundabout are reduced with good design.”

A roundabout is proposed at the Kanan Road/Agoura Road intersection. The Kanan roundabout should be two lanes wide allowing for through traffic in the left lane and free right turns in the right lane. Any pedestrian crossing shall be clearly marked and pedestrian refuge areas should be provided in the approach medians. (Refer to Figure 3.1)

Street Improvements

Agoura Road

Agoura Road at the heart of the Village (between Kanan Road and Cornell Road) shall consist of one 12 ft. travel lane in each direction, an 8 ft. buffer/Class II bike route, a 10 ft. landscaped median in the center, 16 ft. diagonal parking on either one or both sides of the street, depending on available width (Refer to Chapter 6–Streetscape Beautification and Public Infrastructure for typical streetscape improvement design), and expanded sidewalks.

The following describes the design of Agoura Road:

- Median improvements along Agoura Road shall provide a physical separation between travel lanes and a refuge area for pedestrians at crossings.
- Due to safety concerns, a low decorative barrier fence and low shrubs should be placed within the median to deter pedestrians from crossing in unsafe areas. However, pedestrian refuge areas shall be provided at key points in the median, providing these areas are safe.
- Tall canopy trees shall also be planted to help bring the two sides of the street together and create a sense of place. Tree location shall not interfere with traffic sight distances.
- A roundabout should be constructed at Kanan Road to provide a better traffic flow, calm traffic, and provide visual interest to the area.
- An enhanced crossing shall be provided near the existing mid-block crossing as this location provides the best sight distance for approaching motorists.
- The enhanced crossing shall include curb extensions (bulbouts) at either side and a raised median to provide refuge and traffic calming. Enhanced signage, striping and lighting shall also be provided.
- Pedestrian crosswalks shall utilize textured and colored surface treatments to clearly distinguish these areas for pedestrian movement.

The following street sections depict the future road conditions in the Specific Plan area.

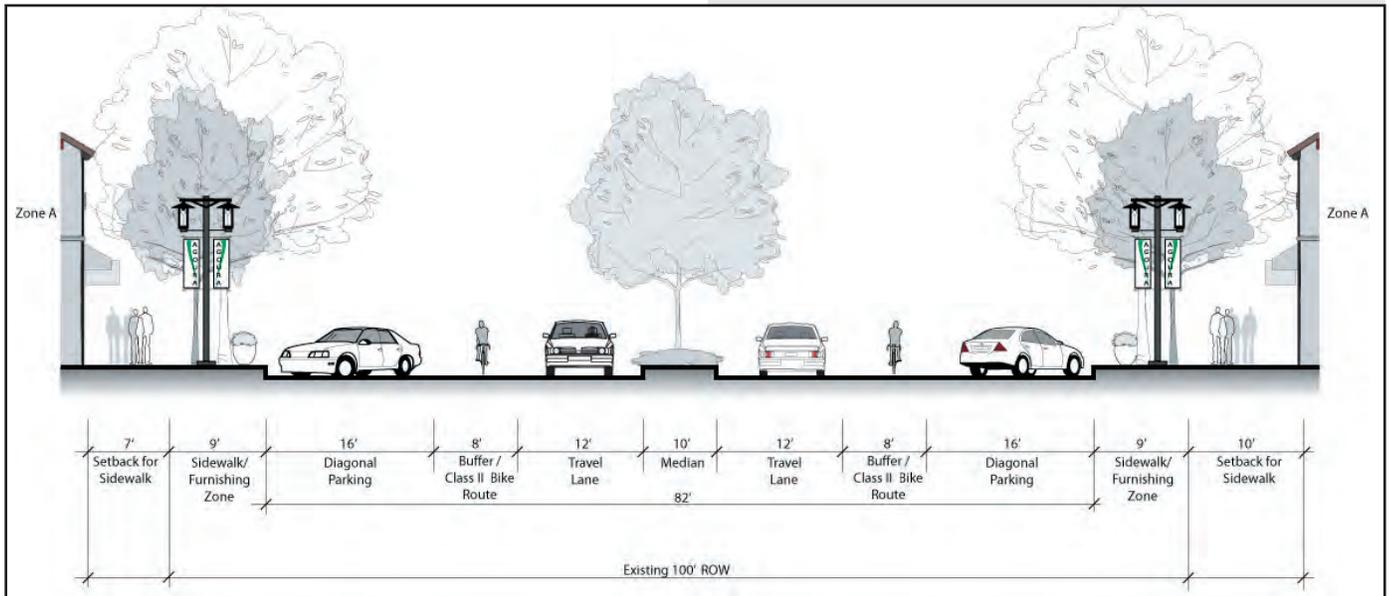


Figure 3.2 - Future Agoura Road street section (between Kanan Road & Cornell Road)

Agoura Road between Kanan and Cornell Roads passes through the heart of Agoura Village. This road is one lane in each direction separated by a 10-foot wide landscaped median. Agoura Road has diagonal parking buffered with a Class II bicycle route.

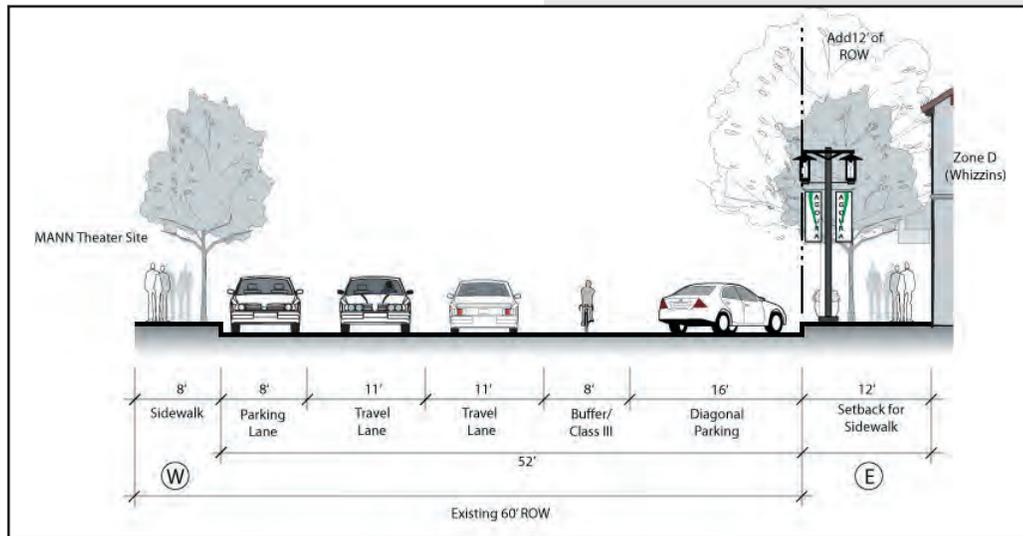


Figure 3.3 - Future Cornell Road street section (north of Agoura Road)

Cornell Road south of Agoura Road is two lanes wide with diagonal parking on the east side and parallel parking on the west. A buffer/Class III bicycle route separates the traffic from the diagonal parking. Twelve feet of additional right-of-way is needed as a setback for the sidewalk on the east side. On the west side a sidewalk bounds the Mann Theater site.

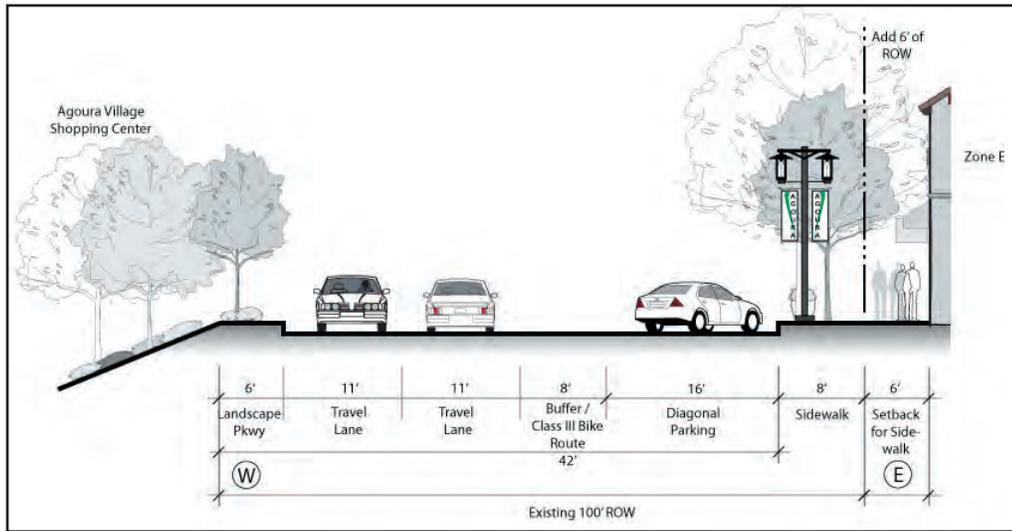


Figure 3.4 - Future Cornell Road street section (south of Agoura Road)

Cornell Road south of Agoura Road is two lanes wide with diagonal parking on the east side. A buffer/Class III bicycle route separates the traffic from the parking. Six feet of additional right-of-way is needed as a setback for the sidewalk on the east side. On the west side a landscaped embankment leads to the Agoura Village Shopping Center.

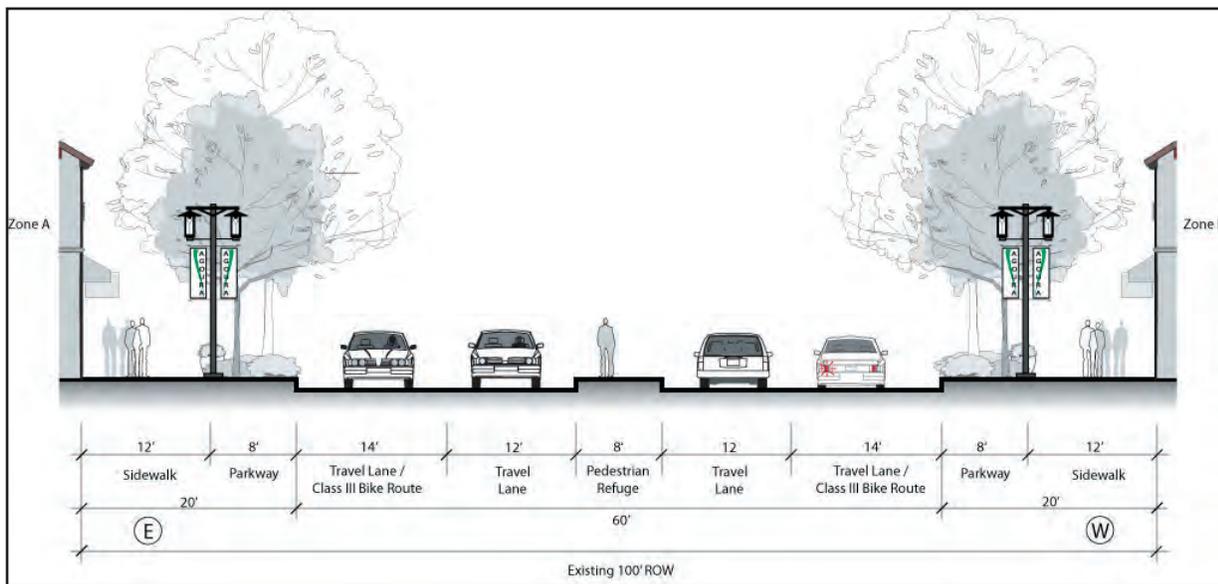


Figure 3.5 - Future Kanan Road street section (at the roundabout)

Kanan Road at the mouth of the roundabout has two lanes in each direction around a middle 'splitter' island. The island diverts the travel lanes to interface with the roundabout and acts as a pedestrian refuge within a crosswalk. The outside lanes are wider to accommodate the circulation of bicycles within the roundabout. These wider lanes transition to a Class II bike lanes farther north along Kanan Road.

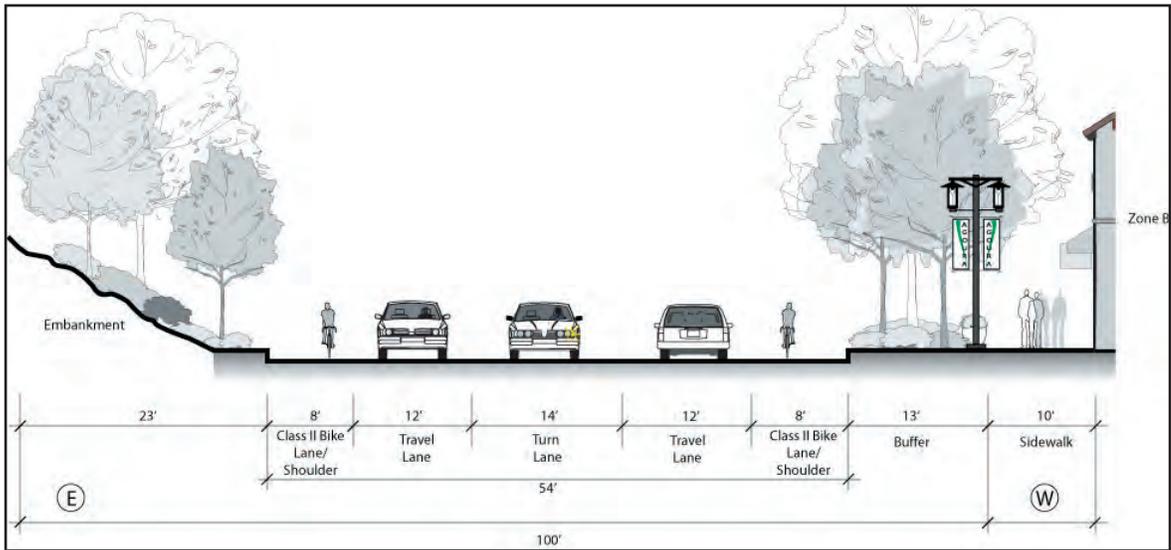


Figure 3.6 - Future Kanan Road street section (south of Agoura Road)

Kanan Road south of Agoura Road narrows to two lanes with a center turn pocket in the middle. A Class II bicycle lane is proposed to flank the travel lanes. On the west side a 13-foot landscaped buffer shall be provided between the curb and sidewalk and a large embankment is located on the east side of Kanan Road.

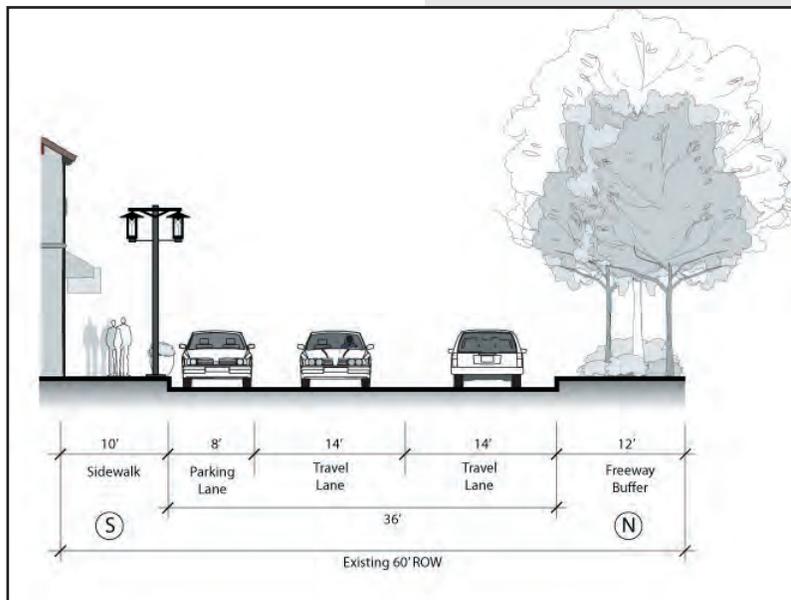


Figure 3.7 - Future Roadside Drive street section (at Whizen Center)

Roadside Drive at Whizen Center is two lanes with curbside parallel parking on the south side. A 12-foot landscape buffer separates Roadside Drive from the freeway on the north side.

Bicycle Circulation

Bike Travel

Alternative modes of transportation such as bicycle use are important in supporting the Village vision.

- The design for Agoura Road shall specifically accommodate bike travel by creating an informal bike space between diagonal parking and the travel lane.
- Agoura Road shall be signed as a Class II bike route. Due to slow vehicle speeds, cyclists will be able to comfortably use Agoura Road as the primary route through this area.
- A Class II bike route shall be established on Kanan Road.
- A Class III bike route shall be established on Cornell Road.

Bicycle Parking

Bicycle racks are important amenities that will encourage bicycle ridership in the Village.

- Bike racks are required as a part of all private development projects in the Village. The preferred bike rack design is provided in Chapter 6 (Streetscape Beautification and Public Improvements).

Bus Stops

Applicants shall provide bus stops within the Specific Plan area. The number to be constructed will be determined in consultation with the City Traffic Engineer and the local transit agencies. Bus stops shall meet the requirements of the transit agency providing service to the City and shall include street furniture that provides shelter for passengers.

Equestrian/Trail Access

One of the unique attributes of Agoura Village is the proximity of the Santa Monica Mountains. Access to this national recreation area from the Village is an important element of the Plan. The Plan also calls for enhancement of the existing Zuma Ridge trail along Agoura Road at Cornell and recommends the development of an equestrian facility on the south side of Medea Creek along Cornell Road.

New development shall plan for, facilitate, and improve pedestrian access trails in accordance with California State Parks trail construction guidelines along the following creeks:

- Chesebro Creek: concurrent with further development, a public trail and restoration landscaping shall be designed and installed along Agoura Road east of Cornell Road.
- Medea Creek: concurrent with further development south of Agoura Road, a public trail and restoration landscaping shall be designed and installed. A study to restore Medea Creek north of Agoura Road to a naturalized creek environment and the feasibility of establishing a bike/pedestrian trail between the Village along Medea Creek, under Highway 101 to the north side, adjacent to Canwood Street, shall be undertaken.
- Lindero Creek: concurrent with further development within the Specific Plan area, a public trail and creek restoration shall be designed and installed.

Parking Strategies

The parking discussion in this chapter addresses an overall parking strategy for the Specific Plan and focuses primarily on public parking. Chapter 4 (Land Use and Development Standards) provides specific regulations and direction for regulating the required amount and location of parking provided for individually proposed developments. Chapter 9 provides information about shared parking options.

The following directions are provided to ensure that efficient and adequate public parking is available in the Plan area:

- New diagonal parking on Agoura and Cornell Roads will provide short-term parking needs for the area while reducing crossing distance for pedestrians.
- The current Los Angeles County Flood Control Maintenance Yard site south of Agoura Road is recommended to be developed for public and employee parking. A pedestrian connection between this public lot and Zone A south is strongly encouraged.
- A shared parking policy is recommended by this Plan.
- A parking study to determine if additional public parking is needed; the quantity and optimal location of public parking; define a parking management strategy; and recommend methods to fund and finance (including an in-lieu fee program) public parking facilities shall be completed as deemed necessary.

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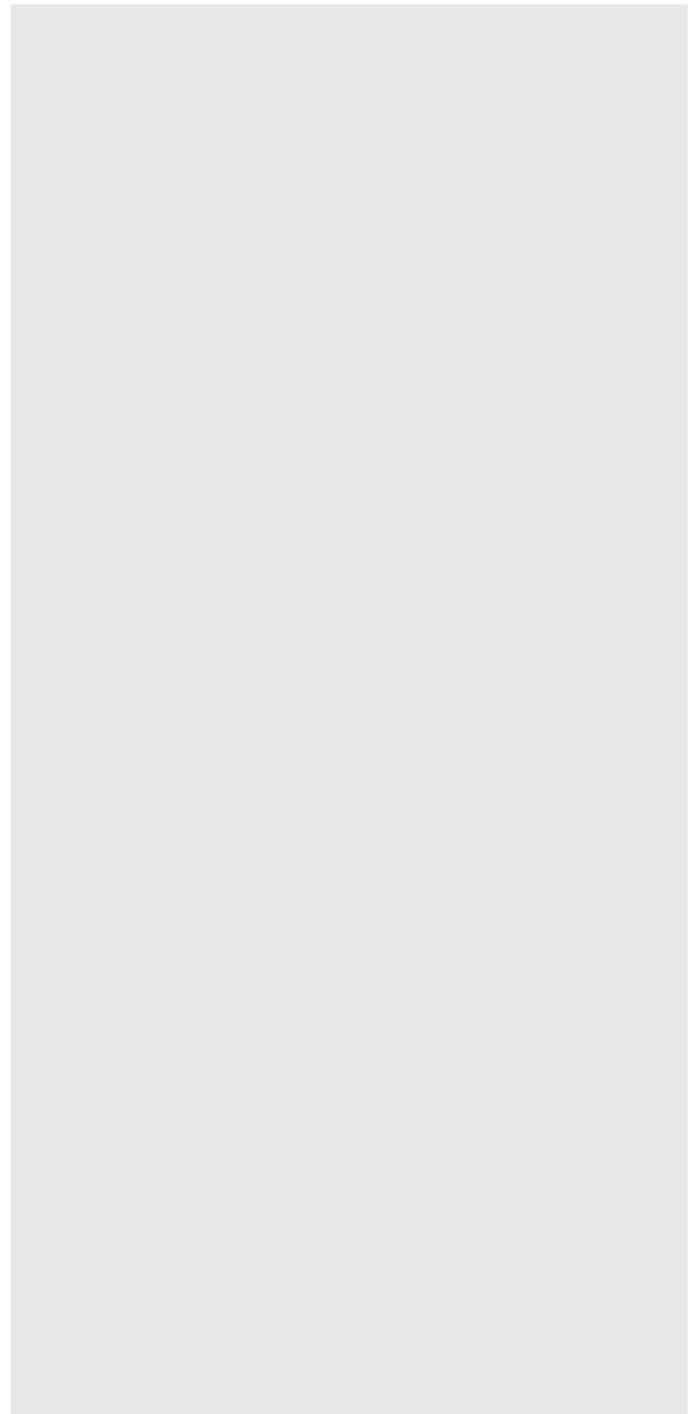
CHAPTER 4: LAND USE AND DEVELOPMENT STANDARDS

Introduction

In concert with design guidelines, streetscape beautification, and other public improvements recommended for the Specific Plan area described in Chapters 5, 6, and 7, standards for new development are intended to reinforce the desired urban image in the area. These standards are also intended to improve overall aesthetic appearance and to serve as the incentive for private investment in the Specific Plan area.

This chapter focuses on regulations and standards for proposed development within the Specific Plan area. These standards are mandatory requirements that must be satisfied for all new buildings, renovations or additions. Non-conforming lots, buildings, uses, and standards shall be governed by Zoning Ordinance Chapter VII (Nonconforming Lots, Uses, Buildings and Land).

Unless otherwise noted in the Specific Plan, permit requirements are as specified in the City of Agoura Hills Zoning Ordinance. When the Plan is silent, then the Agoura Hills Zoning Ordinance requirements remain in force. Where the Specific Plan and the Zoning Ordinance are in conflict, the Specific Plan shall take precedent. Application procedures and processing requirements are provided in Chapter 9, Plan Administration.



Land Uses

Land Use Concepts

The Agoura Village Specific Plan proposes new commercial development (retail, services and office), residential development, and visitor services in both a vertical and horizontal format to create a place that thrives from morning to night.

In some areas a horizontal mix (stand alone residential and stand alone commercial next to each other on a given site) and in other areas a vertical mix of uses (residential or office above retail or commercial within the same building) will be appropriate. Given the size, depth, and topography of some of the lots in the Village, it appears that in some limited instances stand alone residential development may be more reasonable than residential on top of commercial. Moreover, residential development could serve as a transition from commercial development to open space in certain zones. Figure 4.1 below illustrates what a potential mixed-use development within the Village may look like in the future.

Residential development is limited to different types of attached multi-family development, such as apartments and condominiums. Single-family residential is not permitted within Agoura Village because it tends to create a less compact and more suburban atmosphere with the requirement for attached garages and yard areas. Although residential use will be an important component to the success of the Village, it will be a secondary use intended to support the predominantly retail/entertainment focus of the area.

In an effort to guide the development envisioned for the Village, certain types of office space will be restricted to the second floor in specific areas. Small professional office spaces are desirable within the Village. These types of spaces would support the Village concept and contribute to its vitality and economic success.

The Specific Plan has been divided into seven (7) zones (A-G) (see figure 4.2 Development Zone Map) each with unique allowable uses and development standards.

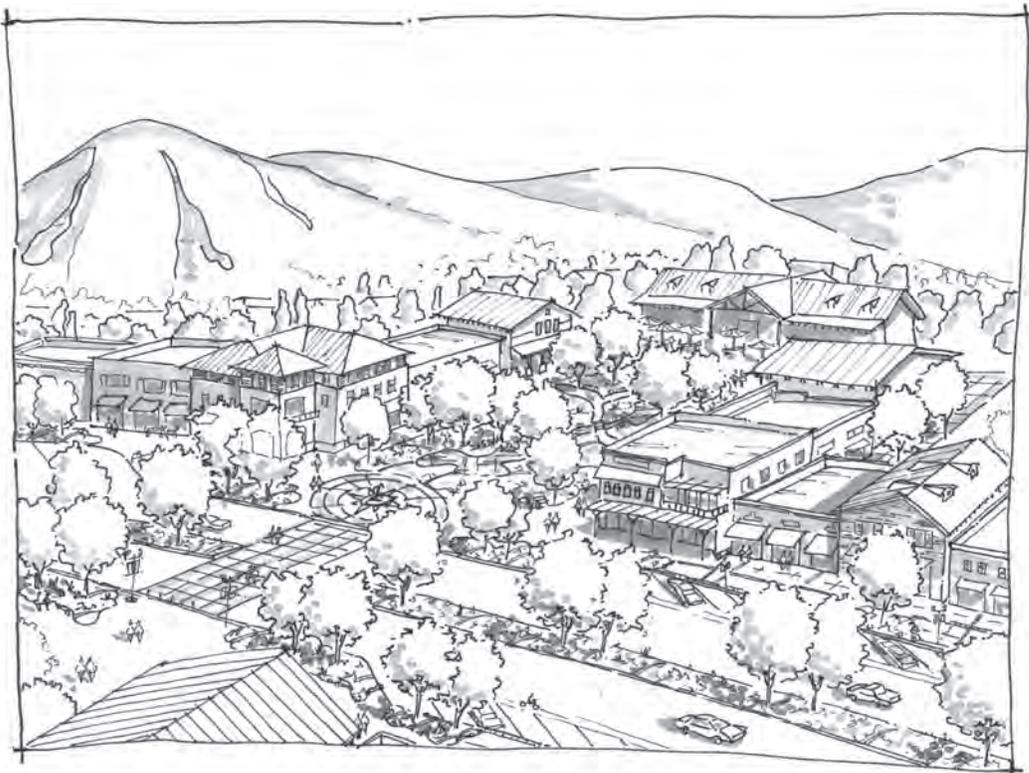
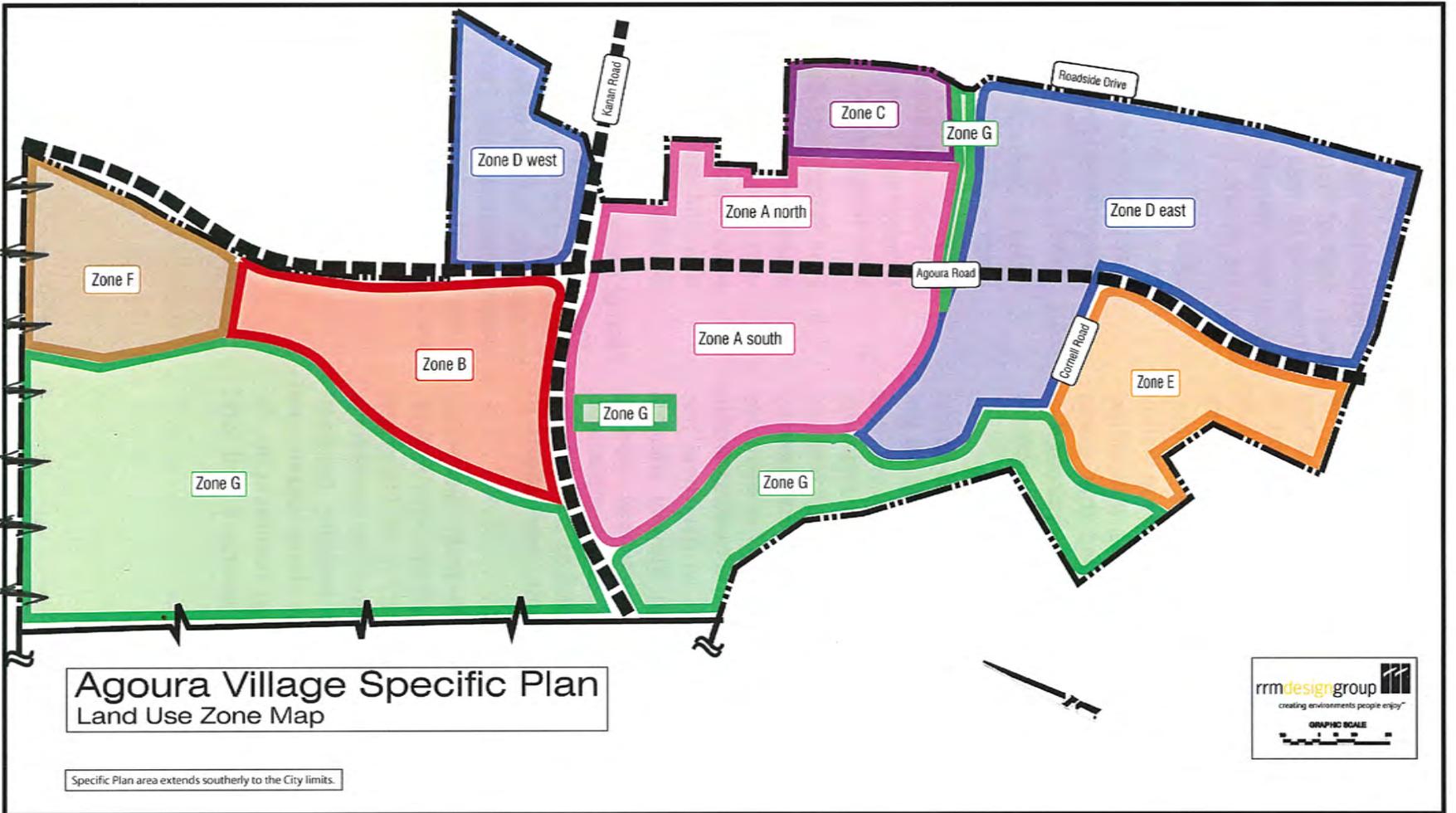


Figure 4.1 - View of potential mixed-use development within the Village

Figure 4.2 Development Zone Map



The “Poly Centric” Agoura Village

Throughout the visioning process and Specific Plan preparation, an effort to focus development activities into one central area has been a key initiative. The purpose of this effort has been to ensure that a destination quality environment evolves with the critical mass of uses necessary to attract people and remain economically viable; thus fulfilling the goals of the Agoura Village Vision. However, due to a number of factors, a modified version of the Village has emerged that would contain several centers where a concentration of retail, entertainment and mixed use development projects could flourish.

The factors influencing this pattern include Kanan Road, the topographical difference between the Kanan/Agoura Road area and Cornell Road and the physical distance between the east and west end of the Plan area. Kanan Road possesses significant village design challenges. It is a heavily traveled road that may not be easily or desirably crossed by pedestrians. While the policies within the Specific Plan are an effort to overcome this issue by requiring pedestrian friendly designs and traffic calming measures, a village type of pedestrian mobility may be difficult to implement. Thus Zone B should be developed as a mixed use, pedestrian oriented village with best efforts to connect it with Zones A and F.

Similar challenges apply to physically connecting Zone A with Zones D and E in a village environment. The significant distance separating the areas and the relatively steep slope between the areas serve to prevent easy pedestrian mobility between the two. Efforts to overcome these issues are in the Specific Plan by making requirements for pedestrian facilities, design treatment and land use distributions.

A realistic approach to successfully overcoming these challenges is to promote the development of several village focus areas within the Specific Plan area. Each of these focus centers could develop a vibrant mix of uses forming an interesting “village within the village” pattern. Three such centers should be planned for:

1. Zone B-West Village: Located in Zone B on the west side of Kanan Road, this area should provide a mix of uses in a pedestrian-oriented village design consistent with the Specific Plan vision.
2. Zone A-Central Village: Located in Zone A north and south of Agoura Road, this central village area will contain the largest amount of retail and entertainment and residential development serving to anchor the east and west village areas.
3. Zone D-East and E-East Village: Located at the intersection of Agoura and Cornell Roads, the east village capitalizes on successful existing uses. This area is relatively flat and provides beautiful natural setting with Medea and Chesebro creeks traversing the area.

Each of these village areas should contain a broad range of uses and activities targeted toward specialty retail, life-style entertainment, visitor-serving and limited residential housing in both vertical and horizontal formats. The mix of uses is key in creating a 24-hour pedestrian oriented destination.

Permitted Uses

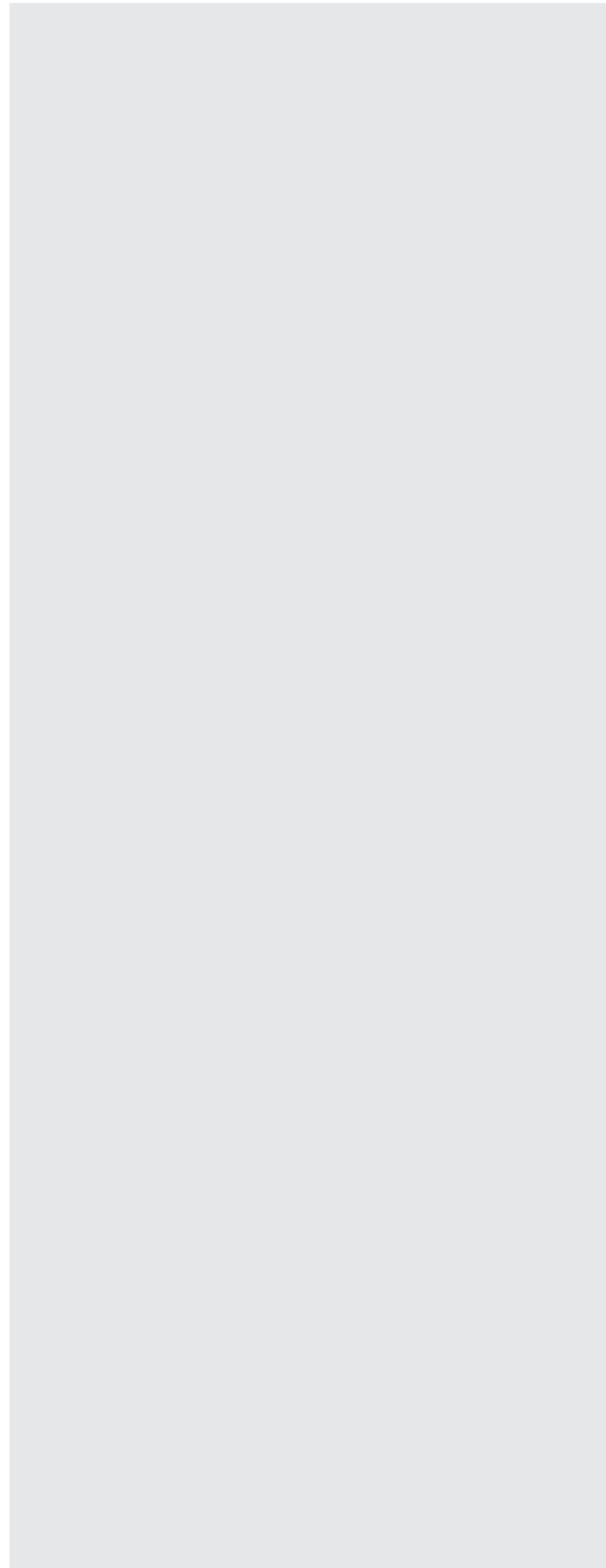
The following Table 4.1 provides a list of permitted and conditionally permitted uses for Zones A-F within the Specific Plan area. Zone G applies regulations pursuant to sections 9481-9489.2 OS - Open Space District and, therefore, is not shown on Table 4.1.

In this table, the letter "X" designates the permitted uses, "X*" indicates a use is permitted only in a specific location within the building (i.e. on the second floor), and "CUP" designates uses permitted subject to review and approval of a Conditional Use Permit by the Planning Commission. Where a dash "-" symbol is shown for a use, that land use classification is not permitted. "TUP" designates uses that are allowed under a Temporary Use Permit issued by the Director of Planning and Community Development. Where a specific use is not listed, the Director of Planning and Community Development has the discretion to allow it if it is found to be compatible with the Vision of the Village (Refer to Chapter 9, Plan Administration).

Further, any new development or renovation of existing property shall require an Agoura Village Development Permit. The purpose of this permit is to provide for orderly development of land in conformance with the vision contemplated by the Specific Plan. Application and processing procedures are described in Chapter 9, Plan Administration.

Prohibited Uses

The following are prohibited in the AVSP Zone: Adult book stores, adult entertainment, adult theater and similar adult uses, outdoor displays, mini-storage facilities and yards, truck and equipment rentals and the like, drive-thru facilities such as restaurants and banks, and off sale liquor establishments except for wine tasting/sales.



Allowable Uses by Proposed Zone

		Zone A	Zone B	Zone C	Zone D	Zone E	Zone F
Residential	X = permitted use						
	X* = permitted use - in key locations						
	CUP = conditional use permit						
	TUP = temporary use permit						
	- = prohibited uses						
Uses							
	Apartments, condominiums, retirement homes, or living quarters (above the ground floors only)	CUP	CUP	-	-	CUP	-
	Stand alone residential (such as apartments, condominiums, retirement homes, or living quarters)	CUP	CUP	-	-	CUP	-
Retail	Antique store	X	X	X	X	X	-
	Art gallery	X	X	X	X	X	-
	Bicycle sales, rent, service	X	X	X	X	X	-
	Beauty supply	X	X	X	X	X	-
	Book store	X	X	X	X	X	-
	Candy store	X	X	X	X	X	-
	Clothing and apparel store	X	X	X	X	X	-
	Costume shop	X	X	X	X	X	-
	Drug store / pharmacy	X	X	X	X	X	-
	Electronic equipment	X	X	X	X	X	-
	Eyewear	X	X	X	X	X	-
	Florist	X	X	X	X	X	-
	Footwear	X	X	X	X	X	-
	Game arcade	CUP	CUP	CUP	CUP	CUP	-
	Gift store / card store	X	X	X	X	X	-
	Hobby shop	X	X	X	X	X	-
	Home decorator shop	X	X	X	X	X	-
	Jewelry store	X	X	X	X	X	-
	Lapidary store	X	X	X	X	X	-
	Luggage store	X	X	X	X	X	-
	Miscellaneous apparel	X	X	X	X	X	-
	Movie theatre	CUP	CUP	CUP	CUP	CUP	-
	Music store (including instrument repair and lessons)	X	X	X	X	X	-
	Newspaper / magazine stand	X	X	X	X	X	-
	Novelty store	X	X	X	X	X	-
	Pet store (no kennel)	X	X	X	X	X	-
	Photo development	X	X	X	X	X	-
	Picture framing shop	X	X	X	X	X	-
	Specialty retail store (consistent with purpose of this district)	X	X	X	X	X	-
	Specialty grocery	X	X	X	X	X	-
	Sporting goods store	X	X	X	X	X	-
	Stamp / coin store	X	X	X	X	X	-
	Stationery	X	X	X	X	X	-
Tailor / dressmaking shop	X	X	X	X	X	-	
Tobacco store	X	X	X	X	X	-	
Toy store	X	X	X	X	X	-	
Variety store	X	X	X	X	X	-	
Watch sales and repair	X	X	X	X	X	-	

Table 4.1 Allowable Use Table

Allowable Uses by Proposed Zone							
X = permitted use	Zone A	Zone B	Zone C	Zone D	Zone E	Zone F	
X* = permitted use - in key locations							
CUP = conditional use permit							
TUP = temporary use permit							
- = prohibited uses							
Uses							
Restaurant	Bakery / pastry	X	X	X	X	X	-
	Bar / tavern	X	X	X	X	X	-
	Coffee shop / coffee house	X	X	X	X	X	-
	Delicatessen	X	X	X	X	X	-
	Family restaurant	X	X	X	X	X	-
	Formal dining	X	X	X	X	X	-
	Ice cream shop	X	X	X	X	X	-
	Juice bar	X	X	X	X	X	-
	Microbrewery	X	X	X	X	X	-
	Pizza parlor	X	X	X	X	X	-
	Wine tasting / Sales	X	X	X	X	X	-
	Sidewalk café	X	X	X	X	X	-
	Specialty foods	X	X	X	X	X	-
Professional Offices/ Services (Permitted above ground level only in Zones A, B, and E)	Accounting office	X*	X*	X	X	X*	X
	Advertising business office	X*	X*	X	X	X*	X
	Architect office	X*	X*	X	X	X*	X
	Art studio	X*	X*	X	X	X*	X
	Consultant office	X*	X*	X	X	X*	X
	Day spa	X*	X*	X	X	X*	X
	Dentist / Medical office	X*	X*	X	X	X*	X
	Financial institution	X*	X*	X	X	X*	X
	Health clubs / Gymnasium	CUP	CUP	CUP	CUP	CUP	CUP
	Interior decorator's office	X*	X*	X	X	X*	X
	Law office	X*	X*	X	X	X*	X
	Photography studio	X*	X*	X	X	X*	X
	Veterinary office (no kennel)	X*	X*	X	X	X*	X
Services/ Miscellaneous	Art festival	TUP	TUP	TUP	TUP	TUP	TUP
	Banquet facility / Catering	-	-	CUP	CUP	CUP	CUP
	Barber shop	X	X	X	X	X	X
	Beauty / nail shop	X	X	X	X	X	X
	Child care center	CUP	CUP	CUP	CUP	CUP	CUP
	Community / Senior center	CUP	CUP	CUP	CUP	CUP	CUP
	Dance studio / Martial arts	CUP	CUP	CUP	CUP	CUP	CUP
	Educational services / Training	X	X	X	X	X	X
	Farmer's market	CUP	CUP	-	CUP	CUP	-
	Hotel / Inn	CUP	CUP	CUP	CUP	CUP	CUP
	Library	CUP	CUP	CUP	CUP	CUP	CUP
	Mailbox services / Photocopy services	X	X	X	X	X	X
	Museum	X	X	X	X	X	X
	Live entertainment including dancing	CUP	CUP	CUP	CUP	CUP	CUP
	Office support (data processing)	X	X	X	X	X	X
	Performing arts center	CUP	CUP	CUP	CUP	CUP	CUP
	Sidewalk vendor	TUP	TUP	TUP	TUP	TUP	TUP
Ticket agency	X	X	X	X	X	X	
Travel agency	X	X	X	X	X	X	

Table 4.1 (Continued) Allowable Use Table

Development Standards

The following development standards will provide directions to shape the urban form within the Specific Plan Area. The corresponding development zone plates provide specific development standards, primary allowable uses, and parking locations and requirements for each individual Zone. These are to be used in tandem with the following development standards descriptions. Flexibility in the application of standards may be considered where a project is proposed that better meets the vision of the Specific Plan. Discretionary review of such requests will be evaluated through the Agoura Village Development Permit process (AVDP) that is described in Chapter 9, Plan Administration.

Building Intensities

Floor Area Ratio

For all non-residential projects, building intensities are regulated by Floor Area Ratio (FAR). Mixed-use projects are regulated by FAR and dwelling units per acre. The number of allowed residential units is in addition to the square footage generated by the non-residential square footage FAR cap and is regulated by the maximum number of units for each zone.

FAR is obtained by dividing the gross floor area of a structure as measured from the outside of the exterior walls, which includes all living spaces and service areas, by the net area of the lot. Net area shall be defined as the total area of the lot, excluding areas in Zone G. Full basements and tuck-under, or non-daylit parking located under the building footprint, are excluded from FAR calculations for commercial and mixed-use development.

Dwelling Units per Acre

Residential dwellings are limited to a maximum number of dwelling units per Zone (refer to Table 4.2 Build-Out Summary). A density bonus increase of twenty five (25) percent of the maximum dwelling units allowed for each Zone may be granted pursuant to the provisions in Chapter 9, Plan Administration. Alternative approaches are

offered. One option is based on State affordable housing density bonus provisions. The second option is based on providing impact fees in excess of the required base fees or extraordinary contribution to public improvements.

Affordable Housing

All development must comply with Redevelopment law which requires that a certain percentage of housing be affordable housing. In addition, the City has an Inclusionary Housing Requirement (Section 9133) that must be adhered to.

Minimum Lot Size

All lots hereafter shall be a minimum of twenty thousand (20,000) square feet in area.

Building Area

Building area means the total square footage contained within a structure having a roof supported by walls or columns.

- Individual buildings shall not exceed thirty thousand (30,000) square feet of gross floor area, so as to promote buildings in a scale appropriate for the Village.
- Buildings that exceed the maximum building area adopted within the Specific Plan area, may be authorized on the recommendation of the Planning and Community Development Director and approval of an AVDP by the Planning Commission. In no case would more than 60,000 square feet be allowed.

Building Coverage

Building coverage means the percentage of the building site covered by structures, open or enclosed.

- Maximum building coverage for both non-residential and residential development allowed under the Specific Plan is sixty (60) percent unless greater coverage is approved through the AVDP process.

				Agoura Village Development Summary (Project)						
Project Zone	Total Zone Area (s.f.)	Developable Envelope (s.f.) ¹	Land Uses within each Zone	Existing	New Development Potential			Full Allowable Buildout		
				s.f.	s.f. ²	DU (base)	DU (bonus)	s.f.	DU ⁷	
A South	600,000	340,000	Retail / Office / Restaurant / Community Center/ Hotel ⁴	-	119,000			119,000		
			Residential over Retail	-		20			20	
			Stand Alone Residential	-		75			75	
								23		23
							Subtotal	119,000	118	
A North	250,000	250,000	Retail / Office / Restaurant	58,192 ³	29,308			87,500		
			Residential over Retail	-		15			15	
								4		4
							Subtotal	87,500	19	
B	700,000	350,000	Retail / Office / Restaurant/Hotel ⁸	-	122,500	50		122,500	50	
			Residential over Retail	-		15			15	
			Stand Alone Residential	-		25			25	
							22		22	
							Subtotal	122,500	112	
C	135,000	135,000	Service Commercial / Office	43,750 ⁵	3,500			47,250		
								Subtotal	47,250	
D West	210,000	210,000	Retail / Office / Restaurant	36,900 ⁶	36,600			73,500		
								Subtotal	73,500	
D East	1,100,000	890,000	Retail / Office / Restaurant	233,200 ³	78,300			311,500		
								Subtotal	311,500	
E	320,000	320,000	Office / Restaurant	-	112,000			112,000		
			Residential over Retail	-		15			15	
			Stand Alone Residential	-		20			20	
							9		9	
							Subtotal	112,000	44	
F	315,000	215,000	Office	-	75,250			75,250		
								Subtotal	75,250	
								Subtotal (Existing)	372,042	0
								Subtotal (New)	576,458	293
								TOTAL =	948,500	293

¹ Developable envelope depicts only the buildable area within each zone. Does not include designated open space areas.

² Based on a FAR = .35 (net) ³ Currently at a FAR of .25 (approx.) ⁴ Total s.f. includes 100-120 room approx. 70K s.f. hotel.

⁵ Currently at a FAR of .30 (approx.) ⁶ Currently at a FAR of .20 (approx.)

⁷ Includes 25% density bonus for contributions to transportation improvement, Agoura Village shared improvement, and/or in-lieu parking fees.

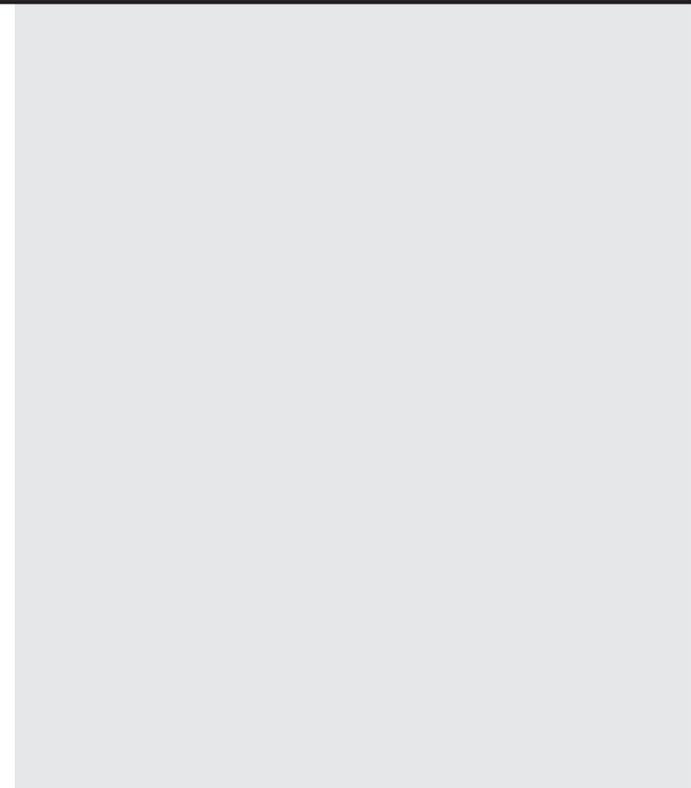
⁸ Option to locate 50 stand alone residential units in place of 100 -120 room approx. 70,000 s.f. hotel.

Table 4.2 Build-out Summary

Projected Build-Out

An important consideration for project review is that for any amount of residential or office space above ground floor retail, the developer of the property must be able to meet all site development standards, such as parking quantity and location, height limitations and design guideline requirements.

The build-out summary (Table 4.2) illustrates the maximum allowable development potential for development within the Specific Plan. This maximum buildout does not take into account environmental constraints or application of all development standards. Also, buildout will occur over time, and thus it is unknown when complete buildout will occur.



Development Phasing for Mixed-Use Projects

Projects must demonstrate that a reasonable amount of commercial space will be developed at the same time as the residential component. The exact quantity and timing shall be subject to the AVDP process. In addition, review of any one project will be reviewed in relationship to the Village concept and adjacent properties to ensure consistency with established goals and objectives of the Agoura Village Specific Plan.

Building Height

Building height is measured using the maximum vertical height of a building or structure at all points measured from natural grade. Architectural elements that do not add floor area to a building, such as chimneys, are not considered a part of the height of a building, but all portions of the roof are included in the building height.

To help strengthen the pedestrian environment and create a balanced street-to-building proportion, building requirements include both minimum and maximum heights. Specific building height requirements for each Zone are described in the corresponding zoning plates.

Buildings that exceed the maximum height adopted within the Specific Plan area may be authorized on the recommendation of the Planning Commission and approval of the City Council through the AVDP process. However, no building shall exceed three (3) stories. A story is defined as that portion of a building between the upper surface of any floor and the upper surface of the floor next above or the roof of the unit. Within the context of dwelling units, the terms "story" and "floor" are synonymous. Refer to the Chapter 9, Plan Administration for description of required findings.

The maximum height of all buildings shall be two (2) stories, or 35 feet, unless at least one floor is dedicated to residential use over a commercial use or the building is a hotel; then, the maximum building height shall be 45' or three (3) stories.

Telecommunication antennas and service structures located on rooftops may also exceed the maximum building height but shall be hidden to the maximum extent possible using appropriate screening and "stealth" technologies and are subject to a CUP.

Project Setbacks

The minimum and maximum setback requirements shall be as follows:

Front Setback

Front setback requirements for each Zone are described in the corresponding development zone plate with the following intent:

- Street fronting buildings should be located at the edge of sidewalk to create a vibrant and walkable Village atmosphere.
- Buildings should be oriented toward the street with parking behind.
- Variable setbacks along Plan area roads will establish spaces for outdoor seating areas, outdoor dining, and shopping. These spaces will enrich the pedestrian experience and provide visual variety within each block.
- When buildings are set back from the sidewalk, allowable uses in the front of the building include plaza or pedestrian spaces, landscaping, retail display areas, and café or outdoor seating areas.
- To help define the corners of street blocks and to create a framework in which the variable setbacks will have an impact on the streetscape, corner buildings shall be located closer to the street.

Side Setbacks

Side setback requirements for each Zone are described in the corresponding zoning plates.

- To help create a continuous built edge along Plan area roads, minimum side setbacks are encouraged.

Rear Setback

Rear setback requirements for each Zone are described in the corresponding zoning plates. In most cases, rear setbacks are not required. However, they can create a transition between adjacent property at the rear of the lot by allowing some open space and landscaping between parcels. Rear setbacks also create a place for vehicle access, utility services, and parking.

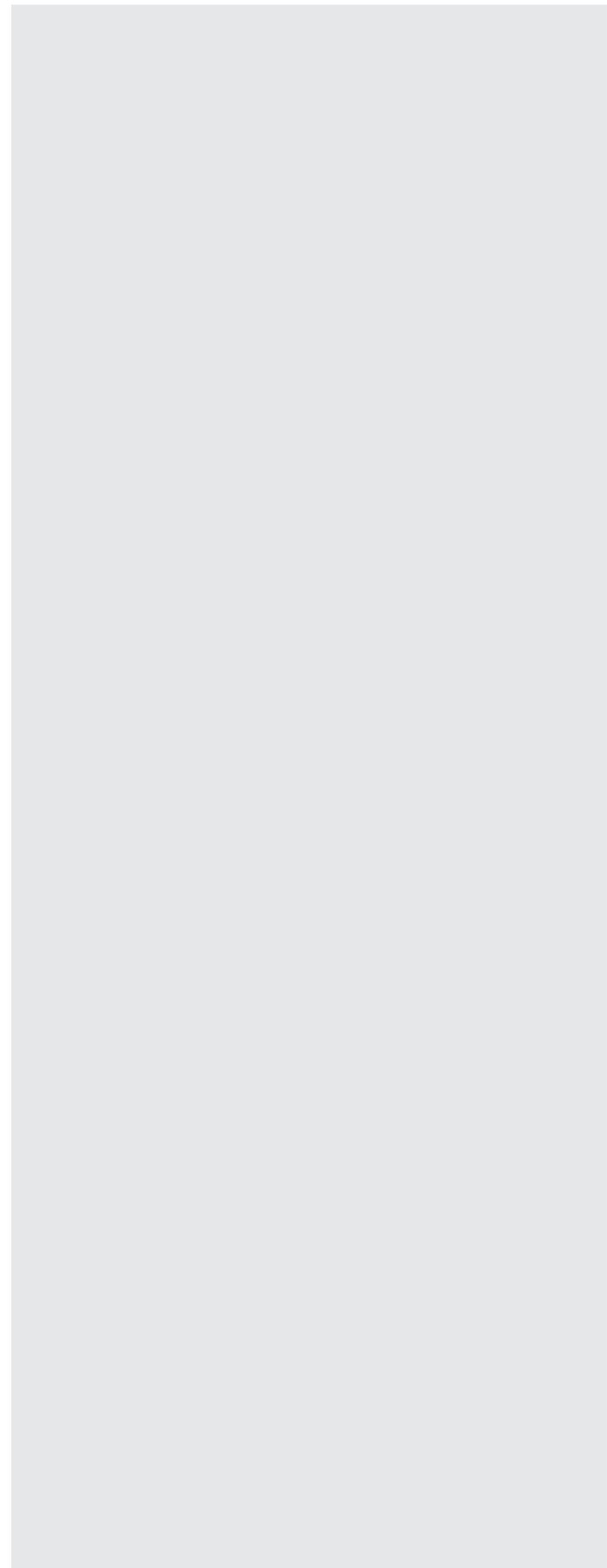
Off-Street Parking

All parking design standards within the Specific Plan area shall be consistent with the standards set forth in Section 9654-9654.5 of the City's Zoning Ordinance, except as noted in Chapter 9.

Location and Access

Off-street parking is not recommended between the building and the street anywhere in the Specific Plan area. In addition, large parking lots should be avoided. Parking should be broken into smaller lots and interspersed around a site. Subterranean parking or at grade parking garages that are "lined" with shops to conceal the parking from public view are highly encouraged. Above ground parking structures shall be designed to contribute positively to the aesthetic quality of the Village and shall be consistent with the architecture of the surrounding buildings.

- Off-street parking is not permitted between the building and the street in Zones A, B, E, and F.
- Parking shall be located at the rear or middle of the lot, with primary access from the side streets, alleys, access easements, or via reciprocal access agreements.
- If there is no feasible side or rear access, then vehicle access to the rear may be taken from the front of the lot, but a maximum of one (1) access point shall be allowed every 100 feet.
- New surface parking lots shall not be located immediately adjacent to public streets except in Zones C and D where parking shall be screened adjacent to street edge. In Zone D, surface parking lots shall not be located immediately adjacent to Cornell Road or Agoura Road.



- Corner properties are encouraged to allow for reciprocal access to interior block properties to provide parking at the rear of the lot.
- Access easements across adjacent lots to the rear of a property shall be arranged on a voluntary basis between individual property owners.

Mixed-Use Parking Reduction

Vertical mixed-use projects may be eligible to receive a reduction in the parking requirements. Refer to Chapter 9, Plan Administration for required findings.

Shared Parking

A shared parking agreement between two or more land uses or if uses are on the same site and have distinctly different hours of operation (e.g. theater vs. office) is encouraged. Refer to Chapter 9, Plan Administration for required findings.

Site Grading and Natural Resources Protection

The City of Agoura Hills is located in the foothills of the Santa Monica Mountains. The community is endowed with natural open space and values natural resources such as creeks, native vegetation, trees, and unique topographical features.

The intent of the following regulations and policies is to integrate natural resources into the planning considerations for proposed developments and to reduce the potential impacts on natural resources within the Specific Plan.

Site Grading

- Development shall relate to the natural surroundings and grading shall be minimized by following the natural contours.
- Graded slopes shall be rounded and contoured to blend with the existing terrain.
- Significant natural vegetation shall be retained and incorporated into the project whenever possible.
- The natural contours of the land shall be respected when developing on sloped properties. Terraced parking lots and stepped

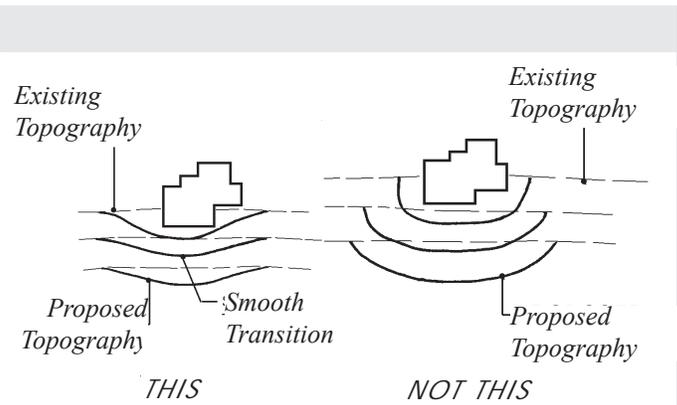
building pads should be used to preserve the general shape of natural landforms and to minimize grade differentials with adjacent streets and with adjoining properties.

- Dwellings should incorporate porches, trellises, landscaping, and other features in the front yard to help soften the transition between the street and the dwelling. When placed correctly, these elements can also provide shading.
- Natural amenities, such as views, mature trees, creeks, riparian corridors, and similar features unique to the site should be preserved.
- All projects shall comply with the City's Grading Ordinance and the Hillside and Significant Ecological Area provisions in the Zoning Ordinance, except for Sections 9652.13 A and B.
- Properties in Zones A South, B, E, and F shall minimize grading on existing slopes and knolls to the greatest extent possible. In addition, earthwork in the area of the knoll located in Zone A South shall be minimized to preserve the scenic quality of the knoll.
- In the event that any proposed retaining walls are visible from designated scenic roadways, the City's Architectural Review Board shall determine whether the walls are consistent with the City's Design Guidelines. Additional design features, such as the use of textured retaining walls with more natural features, such as those that simulate rocks or boulders and/or the planting of landscape vegetation, are encouraged.

Natural Resource Preservation

- Prominent and distinctive natural features of the area shall be preserved and integrated as open space for the use and visual enjoyment of all Village patrons and residents.
- Development shall be clustered on less environmentally sensitive areas of the site to maximize open space and resource protection.
- Oak trees shall be preserved and incorporated into the project whenever possible.

- New developments shall preserve or improve natural conditions on or adjacent to the site such as wildlife habitats, streams, creeks, views, and restore and preserve riparian habitats to a natural state where appropriate,
- For projects adjacent to Medea Creek and Lindero Canyon Creek, it is the developer's responsibility to prepare a riparian habitat and creek protection program and implement it. Such programs shall be prepared by a qualified biologist and shall, to the extent feasible, avoid encroachment into any riparian areas and provide an adequate buffer distance to adjacent development. The buffer shall be at least 50-100 feet from the edge of riparian vegetation of either side of the creek. The specific dimensions will depend on the value of the habitat. Trails and bicycle paths may be allowed in the buffer, depending on the particular design characteristics and on site habitat.
- A transition between development and adjacent open space should be designed to help preserve the rural character of the area. Such transitions may include buffer areas and landscaping to blend development with the surrounding open area.
- Recycled content materials, such as wood substitutes and recycled concrete and asphalt, are encouraged when selecting materials.
- To minimize water borne pollution into local creeks and watersheds, all projects shall adhere to National Pollutant Discharge Elimination System (NPDES) requirements for both construction and on-going operational impacts. Use of bioswales and natural filter systems are encouraged.
- Sustainability practices provided by the US Green Building Council shall be incorporated into all new development projects. Compliance with the provisions will be subject to review and approval of the AVDP by the Planning Commission.
- Solid barrier fencing shall be prohibited around areas that border open space areas or routes



Grading should smoothly transition into the existing contours



Agoura Hills is characterized by abundant oak trees and hillside topography

of animal movement, especially for riparian areas. Fencing in these areas shall consist of “ranch style” post fencing. Fencing shall allow at least one-foot clearance above ground to permit wildlife movement.

- All projects shall comply with the regulations contained in the Oak Tree Preservation Guidelines (Sections 9657 through 9657.5 of the Zoning Ordinance).
- For non-riparian areas, a minimum buffer zone of 50 to 100 feet of native vegetation shall be maintained between urban development and adjacent sensitive native habitats. The specific dimension will depend on the value of the habitat.
- Equestrian trails shall be located no less than 10 to 20 (preferred) feet from the edge of the exterior riparian canopy.
- The landscape palette for each project shall prohibit the use of highly flammable species near areas of open space. Plant selection would be subject to review and approval of the City’s Landscape Consultant.

spaces shall be designed to be integrated into the overall design of new developments, surrounding buildings, and existing open spaces.

- The following areas may be considered as part of the required public space, both individually and in combination:
 - Plazas, paseos, and courtyards
 - View portals and view terraces
 - Required front setbacks
 - Outdoor dining within any of the above
 - Private recreation facilities - provided as part of a town home, condominium, or apartment project
 - Landscaped buffer areas
 - Trails and pedestrian paths
 - Designated landscaped medians and areas created by the developer

If the Director of Planning and Community Development determines that fulfillment of the public space requirements for a specific site would not be feasible or would not substantially further the vision of the Specific Plan, the applicant shall be required to pay an exaction fee to be used by the City to acquire land and develop public space elsewhere within the Village.

Public Space, Parkland, and Trails

Additional open space is an important amenity to make the Village a pleasant and inviting environment for residents, workers, shoppers and visitors. Parkland and trails are also an essential element to providing an enhanced environment and recreational amenities for the Village. Where applicable, pedestrian links, such as hiking trails, bicycle paths, and equestrian trails, should be provided.

Public Space Requirements

To increase the usable open space within the Village, the following will be required:

- All new projects shall include design for public space. Projects are required to provide a minimum of 15% of the net site area for public spaces. Public space may assume a variety of different forms of site design solutions. Public

Park Land and Trails Requirements

- Residents of housing projects shall have safe and efficient access to usable open space, whether public or private, for recreation and social activities.
- A recreational greenbelt and trails system along Medea, Lindero Canyon and Chesebro Creeks shall be constructed to enhance the Village.
- The design and orientation of recreation and open space areas should take advantage of available sunlight and should be sheltered from the noise and traffic of adjacent streets and incompatible uses.

Signage

Each development shall be subject to review and approval of a sign program by the Planning Commission. The sign program shall be designed pursuant to Chapter 5, Design Guidelines in the Specific Plan as well as the City's Sign Design Guidelines and zoning regulations.

Village Zoning Plates

The following pages summarize the development standards for each zone within the Specific Plan area. They are intended to be used in conjunction with the previous text, allowable land use table, and process and findings set forth in Chapter 9, Plan Administration.



Figure 4.3 Development Zone Map

Village Zoning Plate

Zone A Sheet 1 of 2

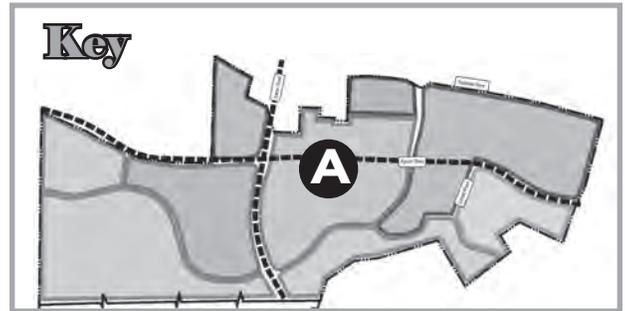
Primary Uses

Non-Residential / Mixed-Use.....Max FAR = .35

Retail, specialty grocery, community center, offices (all types, second floor only), lodging, restaurants, cinema,

Mixed-use (residential/office over retail)

Zone A South = max 20 DU or 25 with Density Bonus,
Zone A North = max 15 DU or 19 with Density Bonus.



Urban Form*

Setbacks (non-residential and mixed-use)

Front (min) – 10' Front (max) – 15'

Side (min) – 0' Side (max) – 10'

Rear (min) – 0'

Building Height

25' (min) - 35' (max) and 2 stories max, unless hotel or residential provided on 3rd floor then 45' max.

Building Area

30,000 s.f. maximum

Building Coverage

60% max

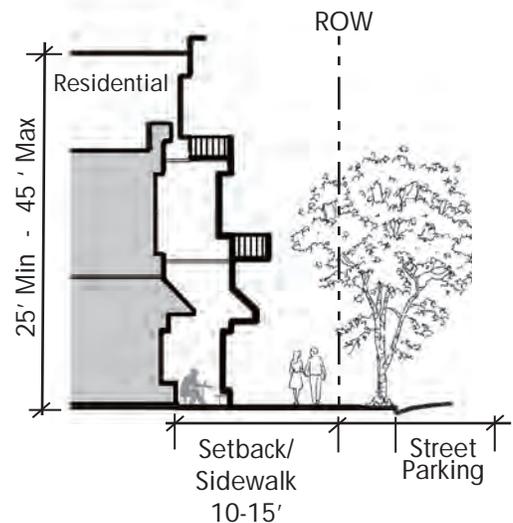
Building Form

Portions of the second and third story shall be recessed from the front facade.

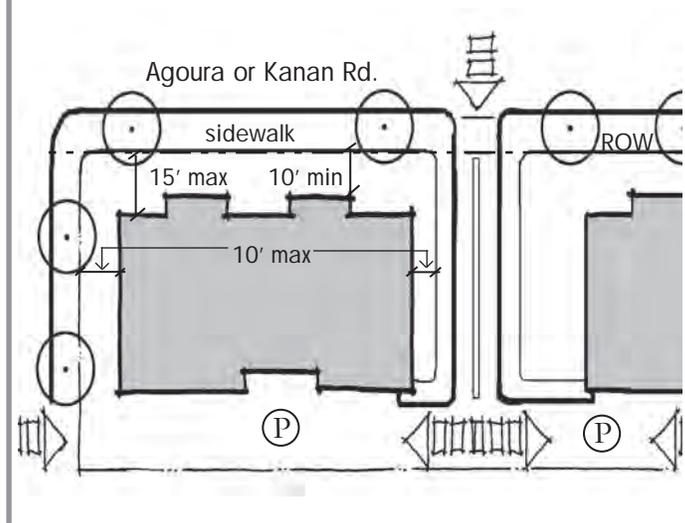
*Some flexibility allowed through AVDP process

Section View

Agoura and Kanan Rd Frontage
Non-Residential / Mixed-Use



Plan View



Parking

Parking Location: No off-street parking allowed between public streets and building. Tuck under, underground, or parking behind only.

Parking Requirements

Per Agoura Hills Zoning Ordinance

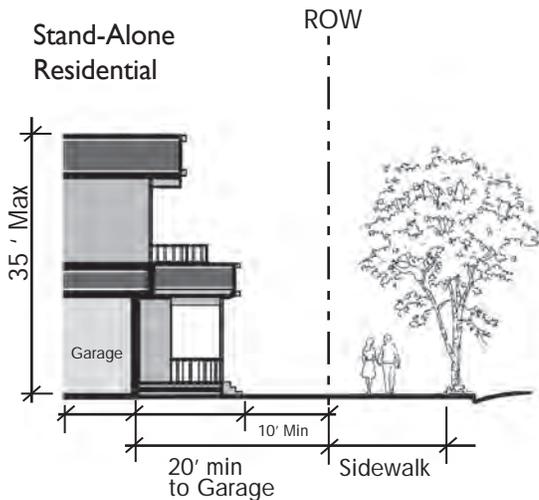
For vertical mixed-use projects, non-residential portion can gain up to a 25% parking reduction with Agoura Village Development Permit (ADVP), see Chapter 9, Plan Administration.

Primary Uses

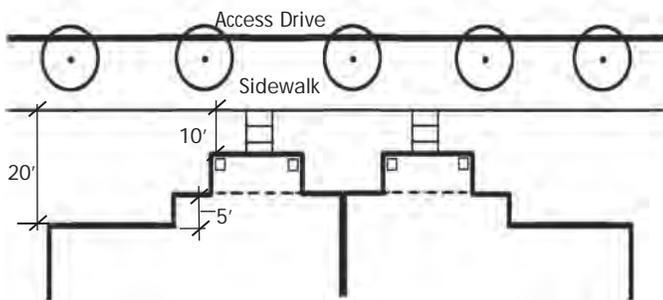
Residential

Zone A South: Stand alone attached residential (not facing Agoura Road or Kanan Road, max 75 DU or 93 DU w/ Density Bonus),

Section View



Plan View



Urban Form*

Setbacks (residential)

Front (min) – 20' to garage Side (min) – 10'
(min) - 10' to front porch Rear (min) – 10'
Garage (min) - 5' behind building face

Building Height

35' (max) / 2 stories max

Building Coverage

60% max

Building Form

Portions of the second story shall be recessed from the front facade.

*Some flexibility allowed through AVDP process

Parking

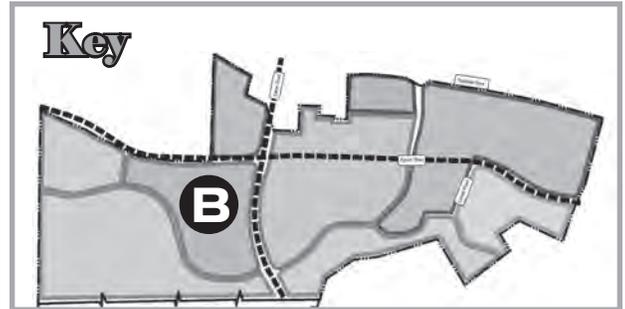
Parking Location: Tuck under, underground, or enclosed.
Access from rear.

Parking Requirements

Per Agoura Hills Zoning Ordinance

Primary Uses

Non-Residential / Mixed-Use.....Max FAR = .35
 Retail, offices (all types), lodging, restaurants, cinema,
 Mixed-use (residential/office over retail) max 15 DU or 19 DU
 with density bonus
 Hotel = 120 rooms or additional stand-alone residential



Urban Form*

Setbacks (non-residential and mixed-use)

Front (min) – 10’ Front (max) – 15’
 Side (min) – 0’ Side (max) – 10’
 Rear (min) – 0’

Building Height

25’ (min) - 35’ (max) and 2 stories max, unless hotel or residential provided on 3rd floor then 45’ max.

Building Area

30,000 s.f. maximum

Building Coverage

60% max

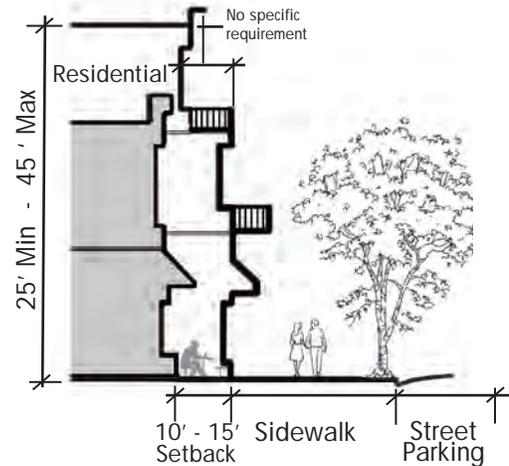
Building Form

Portions of the second and third story shall be recessed from the front facade.

*Some flexibility allowed through AVDP process

Section View

Agoura and Kanan Rd Frontage
 Non-Residential / Mixed-Use



Parking

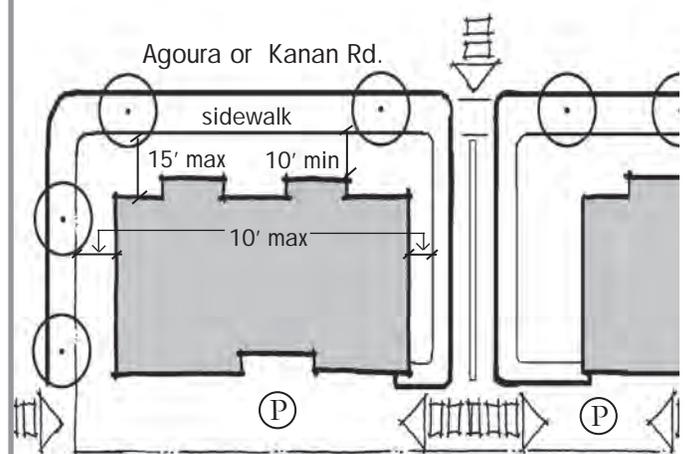
Parking Location: No off-street parking allowed between public streets and building. Tuck under, underground, or parking behind only.

Parking Requirements

Per Agoura Hills Zoning Ordinance

For vertical mixed-use projects, non-residential portion can gain up to a 25% parking reduction with Agoura Village Development Permit (ADVP), see Chapter 9, Plan Administration.

Plan View



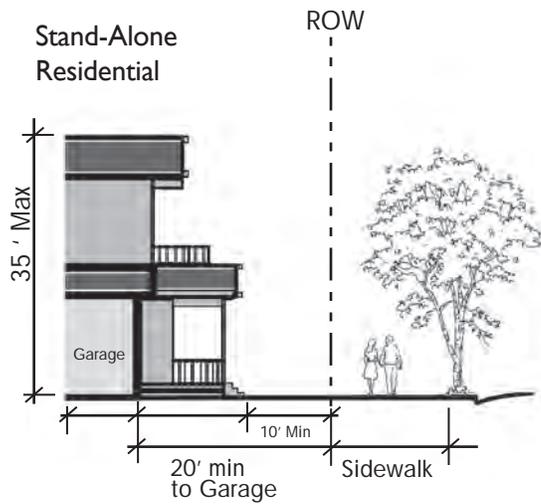
Primary Uses

Residential

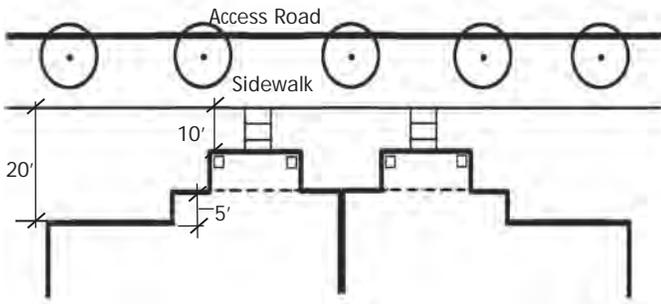
Stand alone attached residential (not facing Agoura Road or Kanan Road, max 25 DU or 31 DU with density bonus)

Additional stand-alone units allowed when hotel is not proposed (max 50 DU or 62 DU with density bonus)

Section View



Plan View



Urban Form*

Setbacks (residential)

Front (min) – 20' to garage Side (min) – 10'
 (min) - 10' to front porch Rear (min) – 10'
 Garage (min) - 5' behind building face

Building Height

35' (max) / 2 stories max

Building Coverage

60% max

Building Form

Portions of the second and third story shall be recessed from the front facade.

*Some flexibility allowed through AVDP process

Parking

Parking Location: Tuck under or enclosed. Access from rear.

Parking Requirements

Per Agoura Hills Zoning Ordinance

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Village Zoning Plate

Primary Uses

Non-Residential.....Max FAR = .35
Commercial services, retail, restaurants, offices (all types)

Residential
Not permitted

Urban Form*

Setbacks

Front (min) – 10'	Front (max) – 20'
Side (min) – 0'	Side (max) – 20'
Rear (min) – 0'	

Building Height

35' (max) / 2 stories max

Building Area

30,000 s.f. max

Building Coverage

60% max

*Some flexibility allowed through AVDP process

Parking

Parking Location: Off-street parking shall be screened adjacent to the street edge.

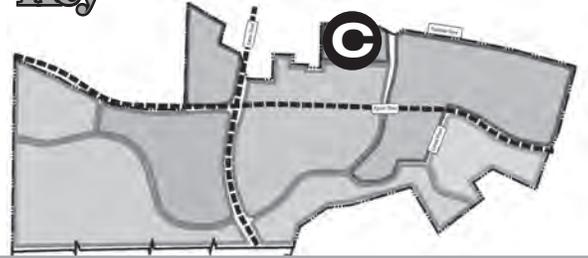
Parking Requirements

Per Agoura Hills Zoning Ordinance

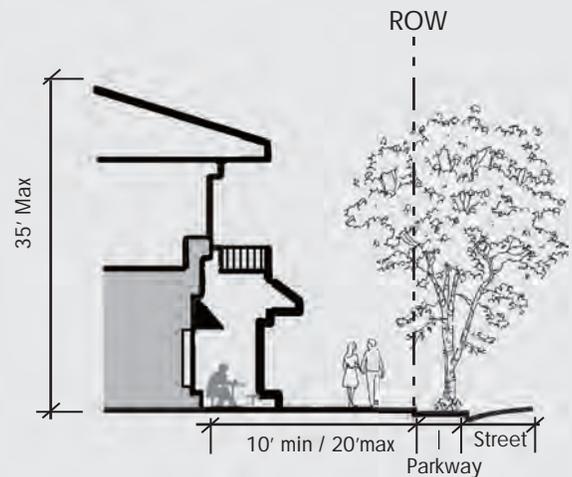
For vertical mixed-use projects, non-residential portion can gain up to a 25% parking reduction with Agoura Village Development Permit (ADVP), see Chapter 9, Plan Administration.

Agoura Village Zone C

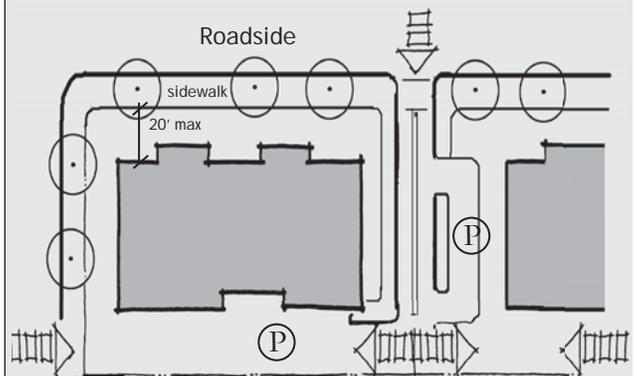
Key



Section View - STREET FRONT



Plan View - STREET FRONT



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Village Zoning Plate

Primary Uses

Non-Residential.....Max FAR = .35
Retail, offices (all types), commercial services, restaurants, movie theatre.

Residential
Not permitted

Urban Form*

Setbacks

Front (min) – 10'	Front (max) – 20'
Side (min) – 0'	Side (max) – 20'
Rear (min) – 0'	

Building Height

35' (max) 2 story max

Building Area

30,000 s.f. max

Building Coverage

60% max

*Some flexibility allowed through AVDP process

Parking

Parking Location: No off-street parking allowed between public streets and building on Cornell Road and Agoura Road. In all other locations, off-street parking shall be screened to the street edge.

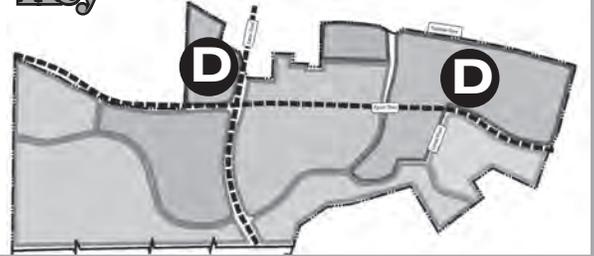
Parking Requirements

Per Agoura Hills Zoning Ordinance

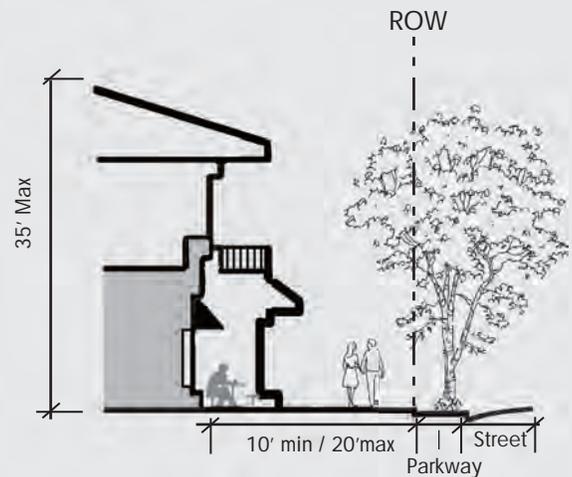
For vertical mixed-use projects, non-residential portion can gain up to a 25% parking reduction with Agoura Village Development Permit (ADVP), see Chapter 9, Plan Administration.

Agoura Village Zone D

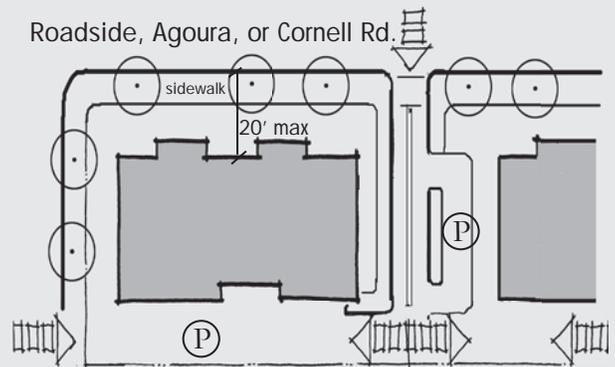
Key



Section View - STREET FRONT

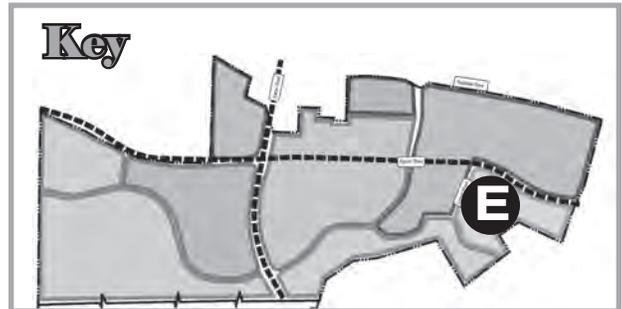


Plan View - STREET FRONT



Primary Uses

Non-Residential / Mixed-Use.....Max FAR = .35
 Retail, offices (all types), lodging,
 Mixed-use (residential/office over retail) 15 DU or 19 DU with density bonus



Urban Form*

Setbacks (non-residential and mixed-use)

Front (min) – 5’ Front (max) – 15’
 Side (min) – 0’ Side (max) – 10’
 Rear (min) – 0’

Building Height

Agoura Road and Cornell Road frontage: 25’ (min) - 35’ (max) and 2 stories max, unless hotel or residential provided on 3rd floor then 45’ max. The height shall be a maximum of 35’ or 2 stories in all other places.

Building Area

30,000 s.f. maximum

Building Coverage

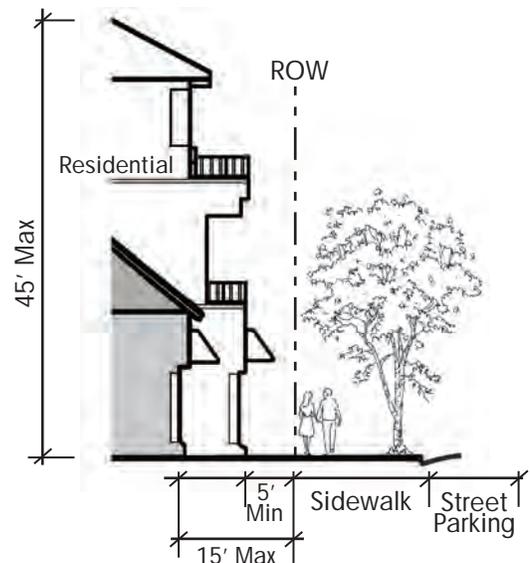
60% max

Building Form

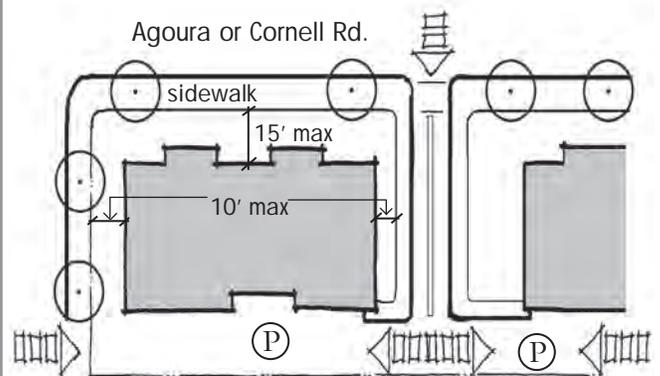
Portions of the second and third story shall be recessed from the front facade.

*Some flexibility allowed through AVDP process

Section View - STREET FRONT



Plan View - STREET FRONT



Parking

Parking Location: No off-street parking allowed between public streets and building. Tuck under, underground, or parking behind only.

Parking Requirements

Per Agoura Hills Zoning Ordinance

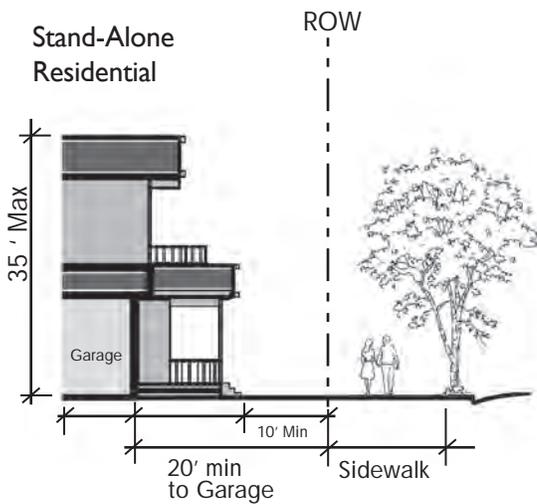
For vertical mixed-use projects, non-residential portion can gain up to a 25% parking reduction with Agoura Village Development Permit (ADVP), see Chapter 9, Plan Administration.

Primary Uses

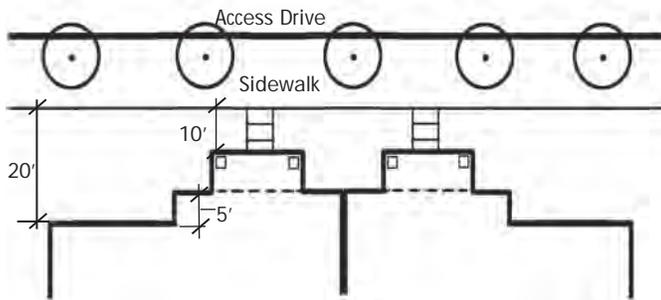
Residential

Stand alone attached residential (non-street fronting, max 20 DU or max 15 DU or 25 DU with Density Bonus)

Section View - STREET FRONT



Plan View - STREET FRONT



Urban Form*

Setbacks (residential)

Front (min) – 20' to garage Side (min) – 10'
(min) - 10' to front porch Rear (min) – 10'
Garage (min) - 5' behind building face

Building Height

35' (max) / 2 stories max

Building Coverage

60% max

Building Form

Portions of the second story shall be recessed from the front facade.

*Some flexibility allowed through AVDP process

Parking

Parking Location: Tuck under or enclosed. Access from rear.

Parking Requirements

Per Agoura Hills Zoning Ordinance

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Village Zoning Plate

Primary Uses

Non-Residential.....Max FAR = .35
Professional office, lodging.

Residential
not permitted.

Urban Form*

Setbacks

Front (min) – 30'
Side (min) – 20'
Rear (min) – 10'

Building Height

35' (max) / 2 stories max, unless hotel then 45' / 3 stories max

Building Area

30,000 s.f. max

Building Coverage

60% max

*Some flexibility allowed through AVDP process

Parking

Parking Location: No off-street parking allowed between public streets and building. Tuck under, underground, or parking behind only.

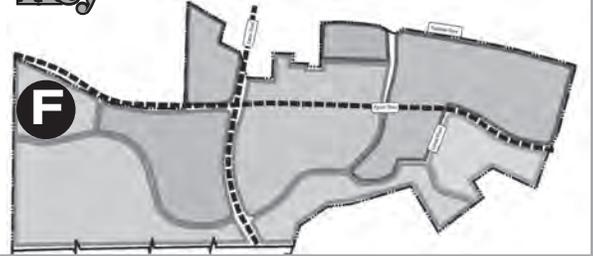
Parking Requirements

Per Agoura Hills Zoning Ordinance

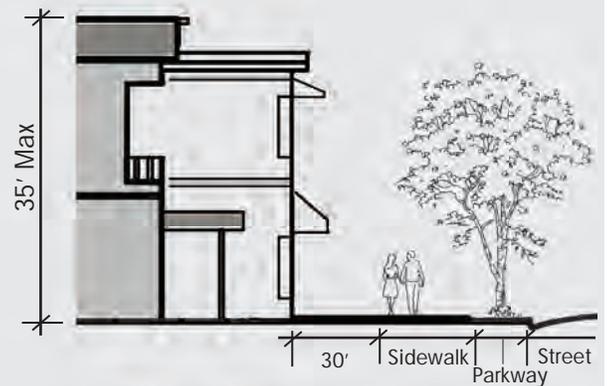
For vertical mixed-use projects, non-residential portion can gain up to a 25% parking reduction with Agoura Village Development Permit (ADVP), see Chapter 9, Plan Administration.

Agoura Village Zone F

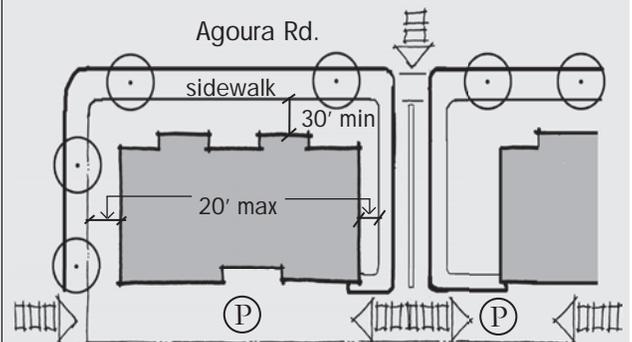
Key



Section View - STREET FRONT



Plan View - STREET FRONT



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CHAPTER 5: DESIGN GUIDELINES

Introduction

The goal of the Design Guidelines chapter is to provide a clear and useful set of design guidelines and criteria for the renovation of existing buildings and construction of new buildings, and to create a truly unique “village” experience in the Specific Plan area. The guidelines will assist City staff, applicants, design professionals, and review bodies in developing an acceptable building design based on the desired scale and pedestrian character of the Village. They are intended to be flexible and allow creativity and variation in the design of buildings that meet the operational business needs of individual property owners, while promoting an overall character and design consistency throughout the Specific Plan area.

Development standards (contained in Chapter 4, Land Use and Development Standards) regulate land uses, lot sizes, setbacks, massing, fencing and off street parking—they inherently act as zoning for the Specific Plan area. Adherence to all development standards is required, except where specific deviations are processed and approved pursuant to the Zoning Ordinance or Chapter 9, Plan Administration. Design guidelines contained in this chapter provide a defined framework of design principles that supplement development standards by providing direction on the more qualitative aspects of a development project. A project may not be required to meet all design guidelines as not all may be applicable on a case-by-case basis. However, projects will be evaluated on the degree to which they demonstrate substantial compliance with the intent of design guidelines, leading to a recommendation of project approval or denial.



Applicability

The City of Agoura Hills may interpret these design guidelines with some flexibility in its review of specific projects. The guidelines will be utilized during the City's design review process to encourage the highest level of design quality, while at the same time providing the flexibility necessary to encourage creativity on the part of the project designers. The overall objective is to ensure that the intent and spirit of the guidelines are followed. Generally, the City will not waive a guideline unless the project designer can demonstrate that such a waiver will facilitate an innovative or otherwise preferable design concept. These guidelines supplement, but do not override, ADA Title 24 and additional requirements set forth in applicable local and State building codes.

Site Planning and Design

Site planning refers to the arrangement of buildings and parking areas, the size and location of pedestrian spaces and landscaping, and how these features relate to one another.

Pedestrian and Vehicular Connections

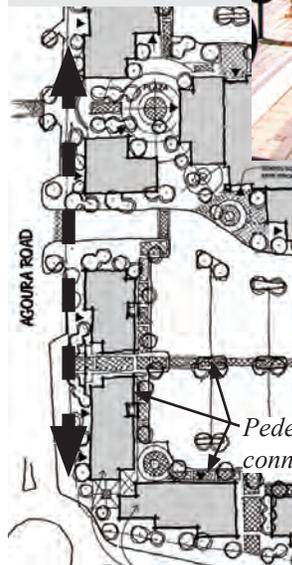
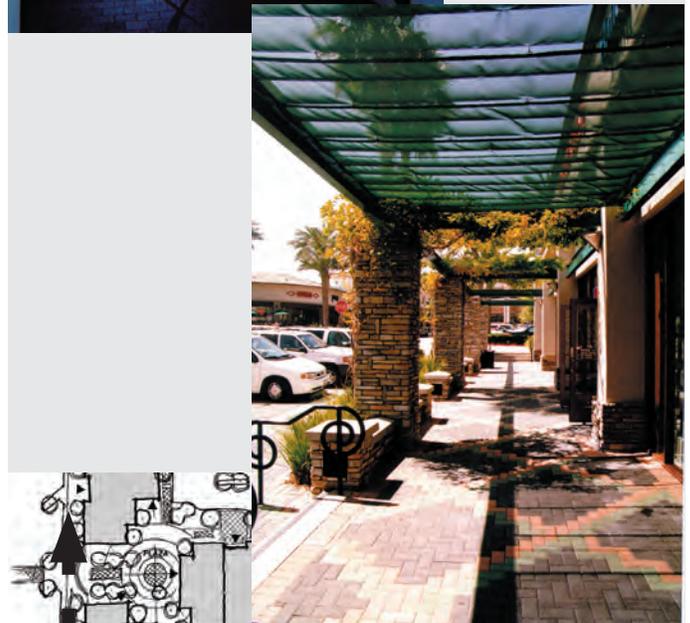
- A. Pedestrian paths or connections should be provided to link individual buildings within the project area and to neighboring properties.
- B. Access between transit/bus stops to building entrances should be clearly defined.
- C. Easily identifiable pedestrian access should be provided from the street/sidewalk to key areas within the site.
- D. Property lines should not be treated as walls and barriers. Buildings should be sited and designed so that there are no barriers or other elements emphasizing property boundaries.
- E. Interface between two or more properties should be considered, i.e. fences should align, landscaping concepts should be complemented, and improvements to the public right-of-way should be seamless.
- F. Where a park-like setting exists adjacent to the creek, meandering paths are encouraged to create a pleasant experience.
- G. Consider paths made from permeable materials such as decomposed granite.

Site layout

- A. Significant buildings with prominent architectural features should be located near corners and intersections whenever possible.
- B. Buildings should be sited close to, and oriented toward, the street. Building design should incorporate covered pedestrian walkways, outdoor seating, and landscape areas where possible. To ensure that the vision of Agoura Road as a main street is realized, buildings along Agoura Road should provide the main pedestrian entrance along the street.



Pedestrian connections should be designed and are often enhanced with special paving



- C. Several small plazas should be located within the Village. Portions of buildings may be set back from the street and alcoves may be provided to include such plazas, entry nooks, and outdoor cafe seating.
- D. Outdoor spaces should have clear purpose that reflects careful planning and are not simply “left over” areas between structures. Such spaces should provide pedestrian amenities, such as shade, benches, fountains, landscaping, public art, etc. Site furniture and light fixtures should reflect the architectural character of the project.
- E. Focal points should be developed to create a definite sense of identification. Plazas, landscape, fountains, artwork, textured pavement, universally accessible changes in pavement levels, and vertical building features may be combined to create focal points and identity.
- F. Whenever possible, new structures should be clustered. This creates plazas or pedestrian malls and prevents long “barrack-like” or continuous rows of structures.
- G. Where possible, utilize courtyards or other methods to break up the building mass and provide natural ventilation.
- H. Site layout should take advantage of the natural environmental setting with the following:
 - provide view sheds from public places
 - orient outdoor / indoor dining and other uses to the creeks and Ladyface Mountain and the Santa Monica Mountains.
 - use natural materials indigenous to the area
 - maintain natural topography in site layout

Parking lot Design and Treatment

Adequate parking within the Village area is necessary for a successful project, however, the parking should not be provided in one large lot.

Parking lots should be broken up into a series of smaller connected lots that are heavily landscaped and connected to buildings with a number of well designed pedestrian paths, trellis paseos, and walkways.

Parking lots within the Village area should be located behind buildings and away from the public right-of-way, and should be clearly identifiable with directional signage located at the street edge.

To this end, these guidelines support the development standards provided in Chapter 4 - Land Use and Development Standards and should provide a parking lot designer with the direction needed to create a functional environment that will blend with the Village area.

- A. Parking areas should be designed so that cars and pedestrians are separated. The need for pedestrians to cross parking aisles should be minimized.
- B. Parking areas should include landscaping, lighting, and pedestrian/vehicular circulation areas.
- C. Parking facilities should be broken into a series of connected smaller parking areas.
- D. Parking lots should provide areas for bicycle and motorcycle parking.
- E. Driveways to parking lots should be located as far from street intersections as possible so that adequate stacking room along the street is maintained. A minimum distance of 100 feet is recommended.
- F. The primary entry drive and pedestrian paths of travel should be designed with enhanced paving, landscaping, and architectural features. Consider using permeable or semi-permeable surfaces, especially near oak trees.

- G. Principal vehicular access should be through an entry drive rather than a parking aisle.
- H. Parking stalls oriented at 90° generally provide the most efficient parking design. However, angled parking is encouraged for large parking lots if it helps to accommodate more landscaping between rows of stalls and at the ends of rows.
- I. Shared parking between adjacent businesses and/or developments is encouraged.
- J. Long rows of parking spaces should be avoided. Instead, parking areas should be broken up with landscaping islands and buildings where feasible.
- K. Parking areas for multi-family residential uses should be located below or not more than two hundred feet away from the unit to be served.
- L. Reciprocal access should be provided so that vehicles are not required to enter the street in order to move from one area to another on the same site and where feasible on adjacent sites.
- M. Large parking areas that service over 100 cars should be designed with a clear hierarchy of circulation: major access drives with no parking, major circulation drives with little or no parking, and parking aisles for direct access to parking spaces.
- N. Trellises, bollards, and other decorative pedestrian amenities shall be provided within parking lots to create pedestrian atmosphere and reduce vehicular visual dominance.
- O. Decorative lighting and landscaping will enhance parking areas and reduce their visual impact on the Village. Shade trees and lamp posts reflecting a craftsman style should be provided.
- P. Light poles, standards, and fixtures shall be a maximum of sixteen (16) feet in height.



Building forms and placement, public art and other focal points, furnishings and light fixtures are all important components that should be considered when laying out a site plan



Landscaped pedestrian access



Parking lots should be generously landscaped

Parking Lot Area Planting

Landscaping within parking lots should be given special consideration. These areas are typically located out of the public right-of-way and should contain different planting materials than a neighboring street.

- A. Trees should be located throughout a parking lot and not merely at the ends of parking rows. A minimum of one tree for every four parking spaces should be provided. Trees should be sized at 24-inch box or larger at the time of installation.
- B. Parking lot trees with 30'- 40' canopies should be planted to shade parked cars and create a more attractive environment.
- C. Landscaping within parking areas should be protected from encroaching vehicles by concrete curbing or raised planting areas.
- D. The height of landscaping adjacent to parking stalls is important to allow the opening of side doors and to allow for vehicle overhang.
- E. Landscape islands should be a minimum of 8' in width to allow for tree growth and to avoid tree trunks from being damaged by cars.
- F. Any portion of the parking area not paved shall be landscaped.



Shade trees in parking lots help to reduce their visual impact and shade parked cars.

Project landscape / Hardscape

All private landscaped areas should strive to establish a rural character, including native, local trees (oaks, sycamores, etc.). Careful maintenance practices should be implemented to achieve a natural appearance. An artificially manicured look is inconsistent with the vision for the Village. Shade trees shall be planted to shade onsite structures to the greatest extent possible in the summer to reduce indoor temperatures and to reduce energy demand.

Plant Materials and Placement

- A. Use of native and low water use plants in conjunction with efficient and long-term water systems for the establishment and maintenance of a drought-tolerant landscape is strongly recommended.
- B. Plant material such as evergreens should be used to soften structural edges, but should not be used to conceal poor architecture.
- C. Water features may be used with planting and natural materials in courtyards and plazas as part of landscaping.
- D. Trees and shrubs should be located and spaced to allow for mature and long-term growth.
- E. Accent planting should be used around entries and key activity hubs.
- F. Formal planting designs and color-spots are encouraged in courtyards and plazas.
- G. Trees should be used to create an intimate scale, enclose spaces, and frame views, but their placement should respect the long-range views of surrounding neighbors.
- H. Trees should be selected on a performance basis with the objective of minimizing water use, providing shade, minimizing hazardous litter, minimizing root intrusion, and providing color and contrast.
- I. Planting should be used to screen or separate less desirable areas from public view, i.e., trash enclosures, parking areas, storage areas, loading areas, and public utilities.

- J. Seasonal shading from trees and shrubs should be considered when developing planting schemes for courtyards and streetscapes.
- K. Plants should be grouped in high and low maintenance and traffic zones and coordinated with irrigation plans.
- L. Due to challenging soil conditions found in the Agoura Village area, extra care should be given to prepare and apply soil amendments prior to planting.
- M. Landscaping planted directly below the eaves or at a rain gutter outlet should be sturdy and have a subsurface matrix of roots to tolerate heavy sheet flow and periodic saturation. Urban runoff can and should be greatly reduced by diverting stormwater from impervious areas, such as roofs and paths to landscaped areas, and infiltration basins where water can seep into the ground.
- N. Implementing measures to reuse rain water and/or grey water for irrigation is strongly encouraged.
- O. Vines and potted plants should be used to provide wall, column, and post texture and color, as well as accentuating entry ways, courtyards and sidewalks.
- P. Use of window boxes is encouraged to provide color-spots, but plants must be accessible for maintenance and should be attached safely and securely.

Paving Treatment

- A. Durable, smooth and even surfaces should be used in well-traveled areas while other materials that have more texture can be used in less traveled areas.
- B. Patterns and colors should be installed in paving treatments using tile, brick, textured concrete in order to provide clear identification of pedestrian access points into buildings, parking features (i.e., handicap spaces, pedestrian loading, etc.)
- C. Utility vaults, such as water meters, gas, and electric, should be architecturally treated to blend with surrounding paving pattern.



Accent paving can be used to enhance and create unique spaces, as well as integrate and screen utility vaults

Water features, ponds, accent landscaping, and creative furnishings all enhance public spaces

Lighting

Effective lighting provides safety and direction for vehicles and pedestrians, visibility and security for businesses, and enhances architectural building and landscape details. These guidelines apply to on-site lighting for private development projects in parking areas, and lights associated with the building and landscaping. Light types could include pole lights, spotlight, wall mounted sconces, parking and landscape lighting.

- A. Sensitivity to the mix of residential / commercial uses, as well as the surrounding hillside areas, should be considered in choosing light sources.
- B. Lighting should be designed to provide ambiance, safety, and security without unnecessary spill over or glare onto adjacent properties. This is particularly important for the residential users who may be located on a second or third floor above a commercial use.
- C. Spotlighting or glare from any site lighting should be shielded from adjacent properties and directed at a specific object or target area. Exposed bulbs should not be used.
- D. Building light fixtures should be designed or selected to be architecturally compatible with the main structure, which should complement the streetscape lighting specified in Chapter 6 - Streetscape Beautification and Public Infrastructure.
- E. Accent lighting that is downlit and focused on key architectural elements and trees can be effective and attractive, however, light sources should be screened from view.
- F. Low-voltage lighting conserves energy and should be used in the landscape whenever possible.
- G. The height of a light pole should be appropriately scaled to the building or complex and the surrounding area. Pedestrian light poles along sidewalks or pathways and parking lot light standards should be 10' to 15' high unless bollards are used.

- H. Landscape lighting can be used to accent walkways and entries and/or seating areas and specimen plants / trees. Landscape lighting should be done with low-level, unobtrusive fixtures and limited to areas of significant landscape resources, such as oak trees and mature trees.
- I. Project design and architectural treatments shall incorporate additional techniques to reduce glare, such as use of low reflectivity glass, use of plant material along the perimeter of structures, brush-polishing metal surfaces, etc.



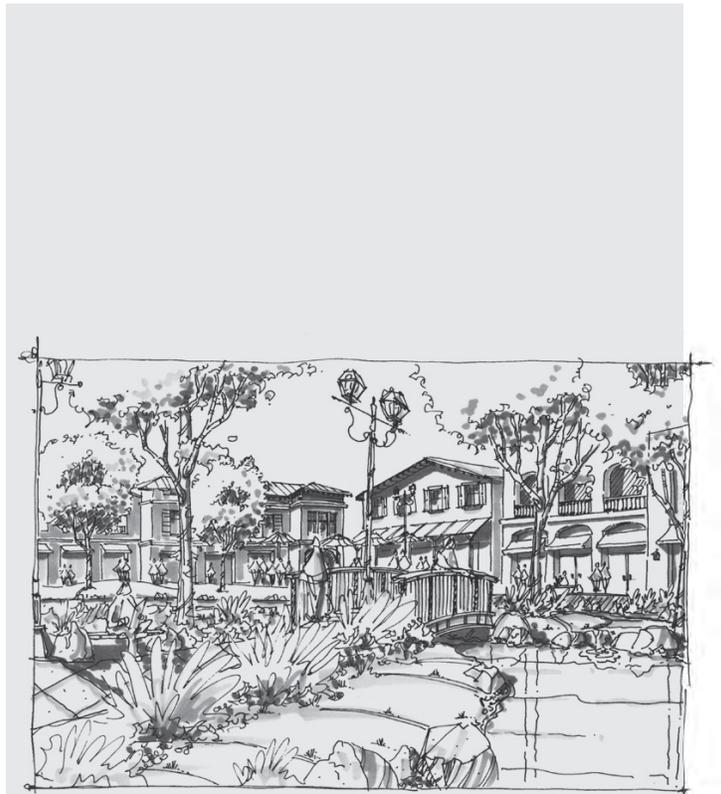
Building and site lighting should complement the architectural character of the development and the streetscape improvements



Building Design Principles

Building forms and facades influence cohesiveness, comfort, and aesthetic pride, and at the same time can encourage shopping, increase a sense of security, and generate pedestrian activity. Where commercial buildings are neighbors to residential buildings, consideration of scale, detail and materials is very important. Any good design must take into consideration some fundamental design principals, including continuity, proportion, mass, scale and rhythm.

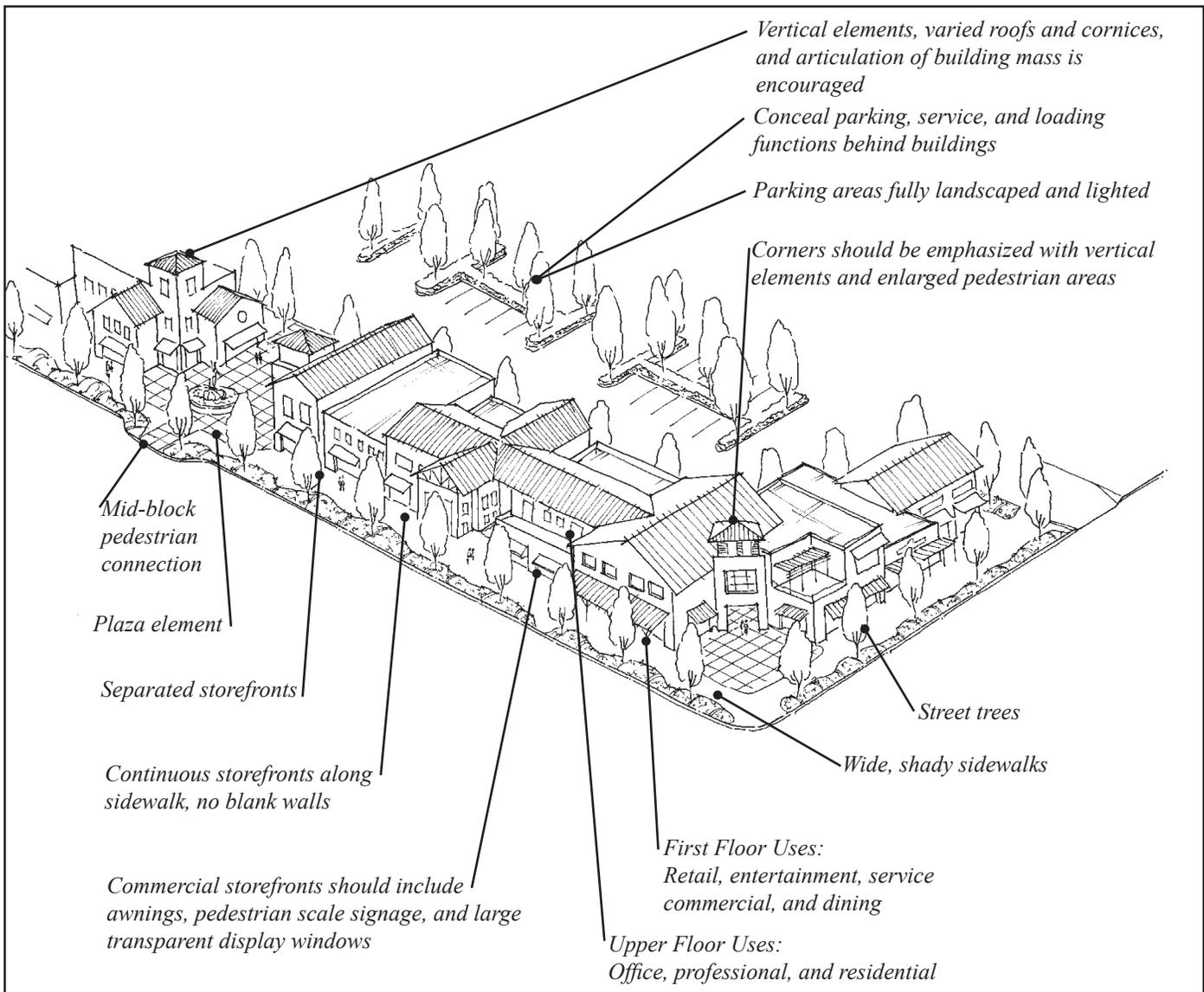
The following guidelines are intended to provide a general framework for design, and do not mandate specific architectural styles, themes or details. The City will be open to considering innovative, alternative design concepts that were not envisioned at the time that these guidelines were written, yet support the intent of this document.



Agoura Village Park / Plaza conceptual design



Conceptual design of multi-family housing in the Village



Good elements of site planning and building form massing contribute to the Village environment

Mixed Use Development

The following guidelines have been developed specifically for mixed-use development within the Specific Plan area. They are to be adhered to in conjunction with the remaining guidelines in the document.

- A. Provide clearly marked parking spaces for each proposed use.
- B. When multiple uses are both proposed in the same building, they should have separate and convenient entrances for each use.
- C. A residential development in a mixed-use project can be benefitted by the addition of a private open space that is only accessed by the residents.
- D. The entire mixed-use development should have a generally consistent architectural style and use of materials. However, if the intent is to differentiate between uses, some architectural details should vary slightly.
- E. Mixed-use projects should minimize use of commercial signage, and only place signs where they are most appropriate for the architectural style of the buildings.

Building Form

Continuity

Continuity among individual buildings in the Specific Plan area contributes to community identity, levels of pedestrian activity, and economic vitality.

As the Specific Plan area develops, each subsequent building project should reflect the existing form, scale, and proportion (relationship of height and width) of the adjacent buildings that helped establish the design context for the area.

- A. New buildings should be no more than one story higher or lower than their neighboring building. If they are, the massing should be stepped so as to create a smooth transition.
- B. Key facade elements on an existing building, (i.e. wall planes and modulation, window and door arrangements, entries, transoms, awnings and cornice treatments) should be repeated in the design of a new neighboring building.
- C. Infill buildings that are much wider than the existing facades should be broken down into a series of appropriate proportioned structural bays or components.
- D. Selection of materials should complement adjacent buildings and their surroundings.

Mass

Mass is defined as a three-dimensional form, such as a cube, box, cylinder, pyramid, and core. The way the forms are sized directly relates to the way building elements are emphasized or de-emphasized. Voids or open spaces in the forms can change their appearance and make the building more interesting and less imposing.

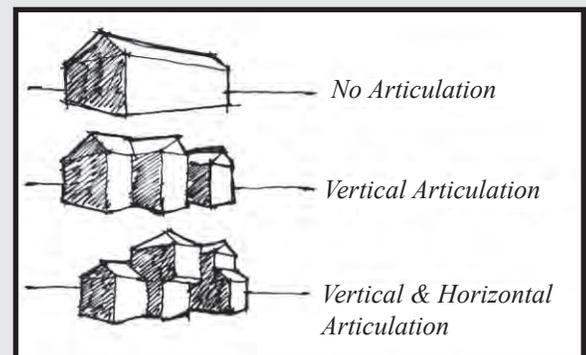
- A. Massing design may include:
 1. Variation in the wall plane (project and recess)
 2. Variation in wall height
 3. Roofs located at different levels



New development height should "transition" from the height of adjacent developments to the maximum height of the proposed structure



Porches, changing wall planes, and different roof forms break up the building massing



Vertical and horizontal building articulation help to break up the building form

- B. Retail spaces should have a 12' minimum plate height at the first floor level to expand the interior volume. Two-story development and massing is strongly encouraged along Agoura Road to bring a comfortable scale to the street.
- C. Each building should have a recognizable base, body, roof line, and entry.
- D. Several smaller buildings rather than one large building should be used to provide an intimate scale and support the Village character.
- E. Changes in vertical planes break up a boxlike appearance. Vertical elements such as pilasters help create "bays" to give the appearance of several smaller buildings.
- F. Surface detailing should not serve as a substitute for distinctive massing.
- G. New development should express its own uniqueness of location, tenant, or structure, designed especially for the particular building site, and not as a copy of a generic building type that might be used anywhere.
- H. The use of corporate "chain" architecture detracts from the unique character of the Village and is strongly discouraged. Corporate tenants should design their buildings to fit the scale and character of the Village.
- I. Varying setbacks on upper floors to accommodate balconies, seating and other architectural treatments should be considered.
- J. Vary the planes of the exterior walls in depth and/or direction. Wall planes should not run in one continuous direction for long distances without a significant offset.
- K. Long, unbroken facades and box-like forms should be avoided. Elements such as balconies, porches, arcades, dormers, and cross gables should be used to add visual interest.
- L. Tall dominating structures should be broken up by creating horizontal emphasis through the use of trim, awnings, eaves, or other ornamentation, and by using a combination of complementary colors or materials.
- M. Recessed or projecting entries and articulation in the storefront mass is encouraged.
- N. Consider the use of narrow floor plan depths to maximize daylight, exterior views, and natural ventilation. Courtyards and atria can also be used to bring light and air into interior spaces.
- O. Consider using several smaller compact building footprints rather than one large footprint to provide an intimate scale and a more efficient envelope to optimize daylight and passive solar heating/cooling functions.
- P. Long, monotonous access balconies and corridors that provide access to five or more units/tenants should be avoided. Instead, access points to units/tenants should be clustered in groups of four or less.
- Q. Combinations of one, one and one half, and two story units should be used to create visual interest and variation in the massing and building height.
- R. Vertical elements, such as towers, should be used to accent horizontal massing and provide visual interest.
- S. Architectural elements that add visual interest, scale and character to the neighborhood, such as balconies, verandas, porches, etc. should be incorporated.
- T. To the extent possible, each housing unit should be individually recognizable. The following methods could be used to break up building massing:
 1. Vary front setbacks within same structure.
 2. Stagger and jog unit planes.
 3. Design a maximum of two (2) adjacent units with identical wall and roof lines.

Scale

Scale is the proportion of one object to another. "Human" or "intimate" scale incorporates building and landscape elements that are modest in size. "Monumental" scale incorporates large or grand building elements. Buildings within the Agoura Village area should incorporate human scale elements.

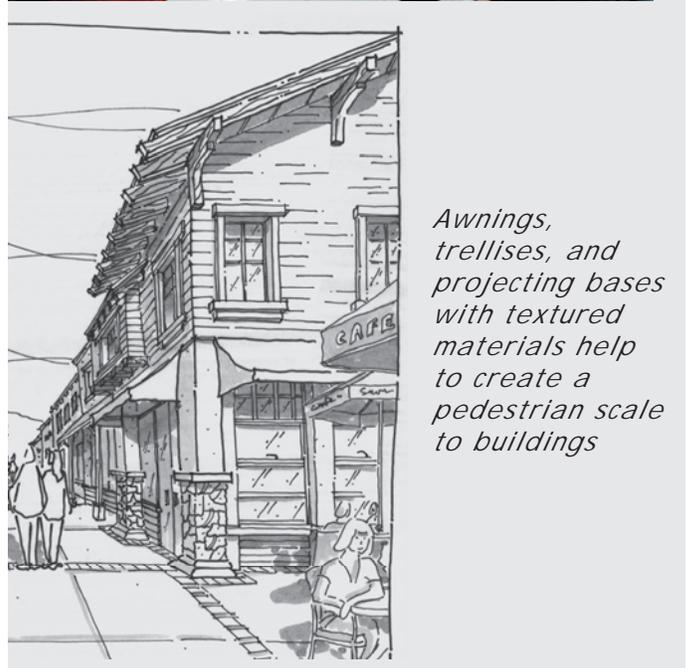
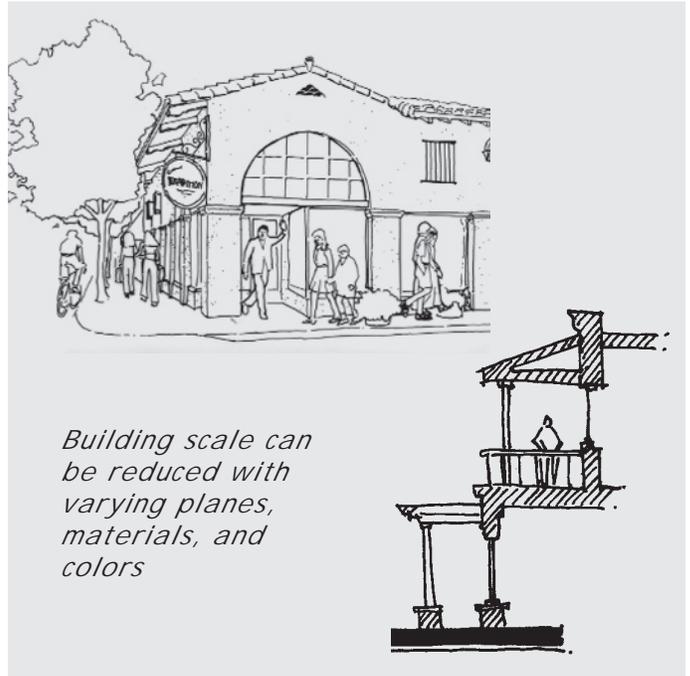
The individual components of the building also have a relationship to each other and the building as a whole, which is the overall scale of building.

- A. Architectural details and materials on lower walls that relate to human scale such as arches, trellises, or awnings should be utilized.
- B. Articulated storefronts with carefully arranged doors, windows, arches, trellises or awnings, rather than blank walls, should face onto pedestrian spaces and streets.
- C. Structures with greater height should include additional setbacks and steps within the massing so as to transition heights from adjacent properties, and to avoid dominating the character of the neighborhood.
- D. Building scale should be reduced through the proper use of window patterns, structural bays, roof overhangs, siding, awnings, moldings, pilasters, fixtures and other details.

Rhythm

Rhythm describes the relationship of building components, as well as the relationship of individual buildings to one another.

- A. Rhythms should be more complex than simply the repetition of one or more architectural details.
- B. Traditional horizontal rhythm intervals generally do not exceed 25 to 30 feet at the ground level, irrespective of a building's total width. Continuation of this familiar, human-scaled rhythm is encouraged in new construction.
- C. Rhythm may be expressed by changing materials or color.
- D. Rhythm may be expressed by using elements such as columns and pilasters.



360 Degree Architecture

The concept of 360 degree architecture is to design a building where all sides of the building have been detailed to be complementary in architecture, massing, and materials the primary street elevation. In other words, the building should be aesthetically pleasing from all angles. This is most important for buildings or corner lots and on elevations that have high visibility.

- A. Buildings located at key intersections should incorporate special architectural elements that create an emphasis on the importance of that location. Such elements may include vertical projections, i.e., clock towers, diagonal walls at the corner, taller, prominent rooftop elements, and/or a substantial art form or fountain.



Example of a long facade broken up into a traditional horizontal rhythm interval



Storefront windows with displays improve otherwise blank walls at the sides and backs of commercial buildings

- B. Blank walls on visible facades are strongly discouraged. Consider utilizing windows, trellises, wall articulation, arcades, changes in materials, or other features.
- C. Murals, trellises, or vines should be placed on large expanses of walls at the rear or sides of buildings to soften and create interest.
- D. Marquis display cases may be provided between buildings in pedestrian linkage areas to eliminate large blank wall surfaces. Such display cases may include theater movie posters, upcoming civic events, retail events (such as sidewalk sales, book signing, etc.), art displays or shows.
- E. Wall mounted lighting should be provided between buildings to ensure security.



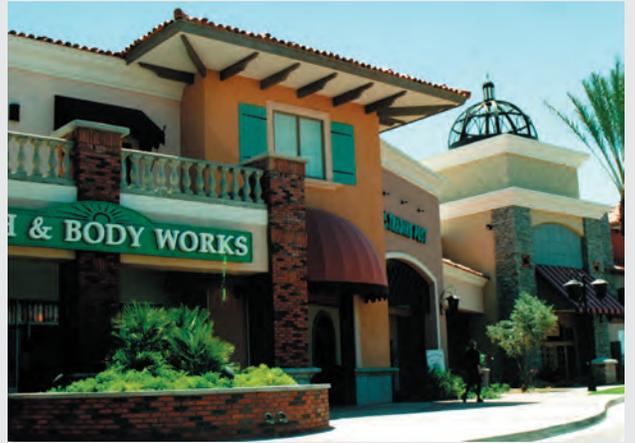
Example of the back of a retail center that has been architecturally enhanced similar to the front facade



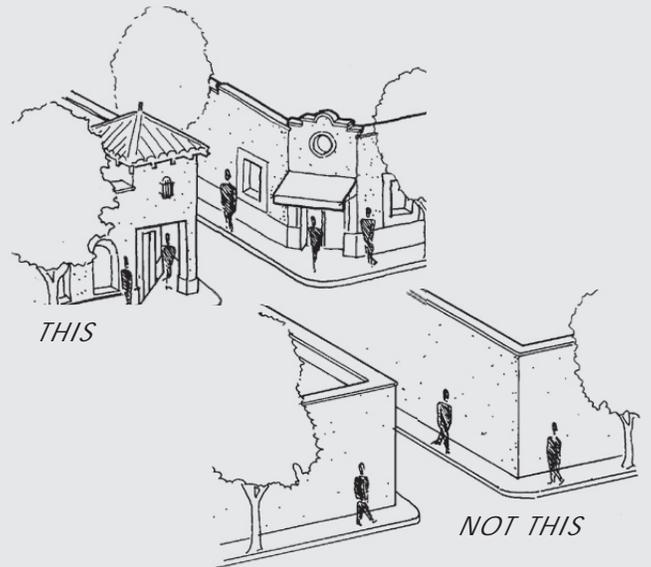
Variation in building massing and articulation should occur on all sides of the building

Roof forms

- A. Multi-form roofs, gables, and shed roof combinations are encouraged to create an interesting and varying roof form that will lessen the mass of the building, and add visual appeal.
- B. Roof materials and colors are important aspects of the overall building design. They should be consistent with the desired architecture and complement adjacent structures.
- C. Deep roof overhangs are encouraged to create pedestrian arcades, verandas, and passive solar benefits.
- D. Roof lines should be varied in height, and long horizontal roof lines should be broken up.
- E. Roofs covering the entire building, such as hips and gables, are preferred over mansard roofs.



Different roof forms and heights break up the massing of the buildings



Raised parapets and roof forms should be used to accent entries and corner properties

Reduce building mass at edges

Various roof heights with gables and dormers



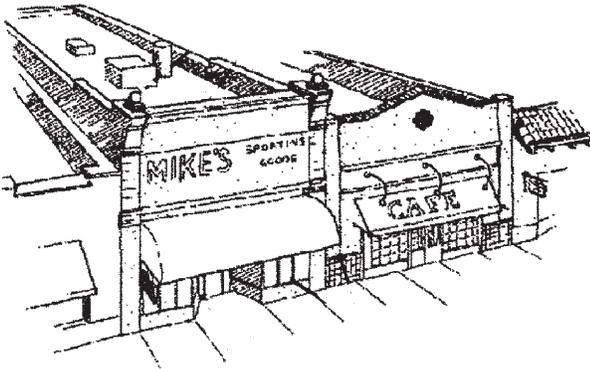
Delineation of individual units within building

Entry Feature

Example of a multi-family building balconies, porches, arcades, dormers, and varied wall and roof planes

Parapets

- A. Parapets should have sufficient articulation of detail, such as precast treatments, continuous banding or projecting cornices, lentels, caps, corner details, or variety in pitch (sculpted).
- B. Rooftop equipment on flat roofs should be screened from public view. Buildings with flat or low-pitched roofs should incorporate parapets, pitched facades, or architectural elements designed to screen roof mounted mechanical equipment and to be architecturally compatible with the design of the building facade.
- C. Parapets should not appear “tacked on” and should convey a sense of permanence.
- D. If the interior side of a parapet is visible from the pedestrian/ motorist area of the project, it should receive appropriate detail, and proper application of materials should be utilized.
- E. Flat roofs should be light in color to reduce solar heat gain.
- F. Commercial rooftops can provide excellent surfaces to install photo voltaic solar panels. Consider producing a percentage of buildings uses on site.



Parapets should not appear “tacked-on” and should screen mechanical equipment

Building Elements and Articulation

Entries and Doorways

It is important that the main entrance to a building is clearly identifiable and unique. It is the primary point of arrival and should be treated as such.

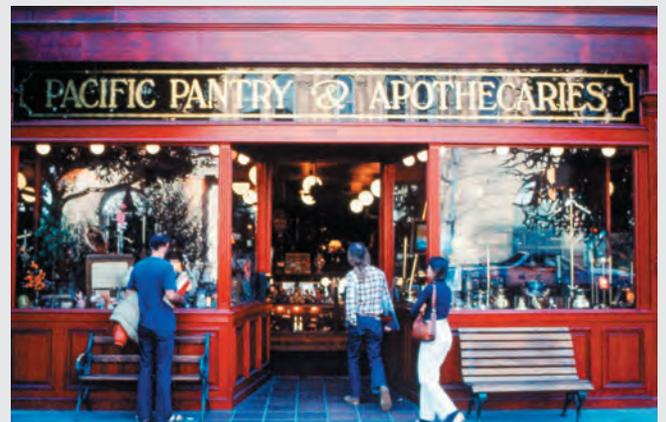
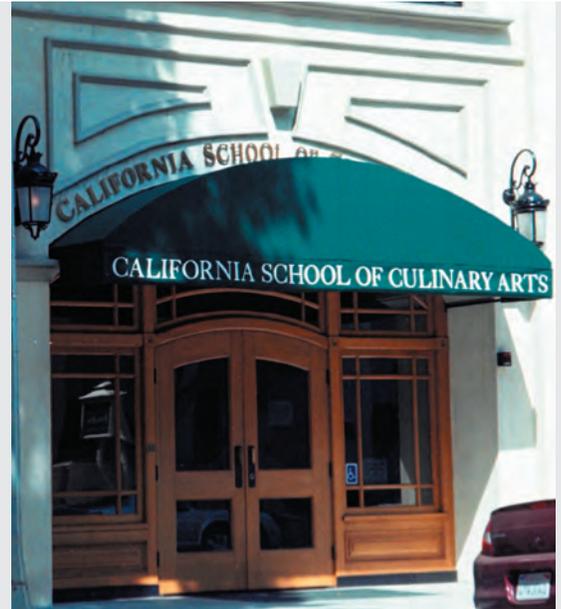
- A. One or more of the following methods should be incorporated in the entrance design.
 - 1. A change in wall / window plane.
 - 2. Wall articulation around the door and projecting beyond the door.
 - 3. Placement of art or decorative detailing at the entry.
 - 4. A projecting element above the entrance.
 - 5. A change in material or detailing.
 - 6. Implementation of architectural elements such as flanked columns or decorative fixtures.
 - 7. Recessed doors, archways, or cased openings.
 - 8. A portico or formal porch projecting from or set into the surface.
 - 9. Changes in the roofline, a tower, or a break in the surface to the subject wall.
- B. Upper floor entries at the street frontage should have their own distinct design that complements the main building frontage.



Example of entrance accented with a change in wall and roof plane

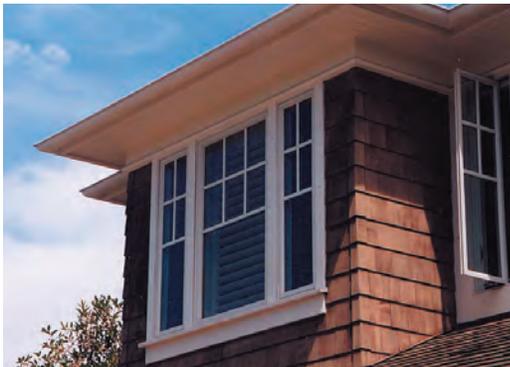
Windows and Doors

- A. At the street level, windows should have pedestrian scale and detail. The framing provides opportunity for color variation and detail.
- B. Clear glass is recommended on street level to create interesting interior shop views for pedestrians. Heat gain can be limited by incorporating awnings, recessed storefronts, polarized glass, or professionally applied UV film. Reflective, mirrored or tinted glass is strongly discouraged.
- C. Storefront windows and doors should be of the same style. The line established by uniform storefront heights helps to establish a sense of scale for pedestrians.
- D. Windows on upper floors should relate to the window pattern established on the ground floor.
- E. Windows and doors should be in scale with the building elevation on which they appear.
- F. Recessed openings, windows and doors provide depth and should be used to break up the apparent mass of a large wall.
- G. Windows may be combined with wall planters at their base or use a base material that has color and texture such as ceramic tiles, brick, stone, or raised wood panels.
- H. Well-designed storefronts, including windows, doors, wall composition, colors, and materials are very important to create a sense of entry and pedestrian scale.
- I. Awnings, landscaping, spectrally selective glass, and controllable blinds should be considered to reduce heat gain through windows.
- J. Window type, material, shape, and proportion should complement the architectural style of the building entry.



Examples of clearly identifiable entries and storefronts

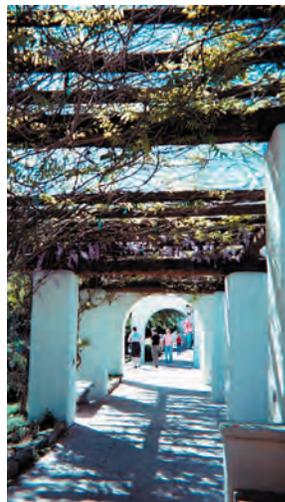
- K. Retail storefronts with display windows are encouraged within the creatively designed façade. Large expanses of glass, glass curtain walls, or glass buildings are discouraged.
- L. Windows and doors should be designed as accent elements with details such as shutters, moldings, and divided lights.
- M. Windows should be located to maximize daylighting and views.
- N. The addition of window articulation such as sills, trim, kickers, shutters, or awnings should be included to improve the building facades where consistent with the desired architectural style.



Windows should reflect the architectural style of the project and should be in scale with the building elevation

Arcades, Porches and Covered Walkways

- A. Covered walkways provide a visual, as well as protective, linkage between uses. These walkways may occur at building street frontages, between buildings, from buildings to parking lots, and within a parking lot.
- B. Covered walkways associated with buildings should utilize the materials of that building.
- C. Archways and columns may also be used to accent store entries and courtyard entries. Materials used should complement the building(s) it is associated with.
- D. Walkways that are “enclosed” by buildings provide an opportunity for pedestrian scale wall treatments such as murals, alcoves, or vines.

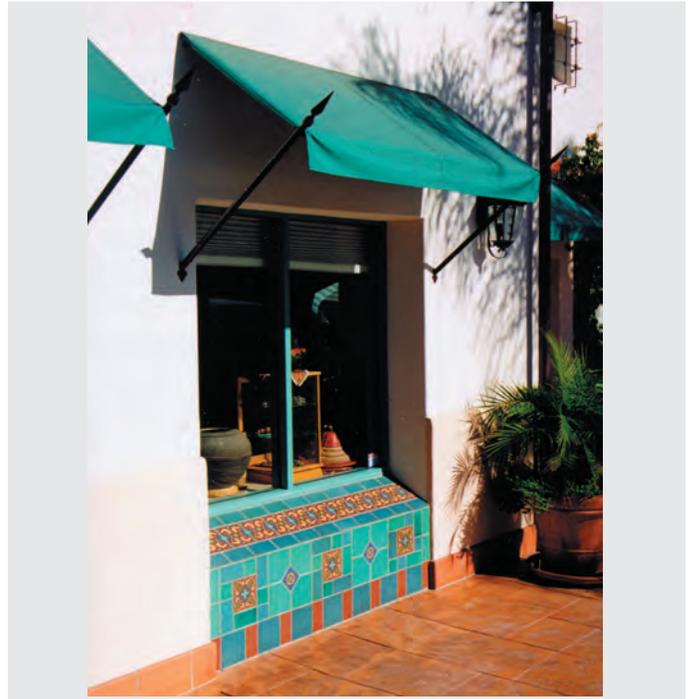


Examples of covered walkways, arcades, and porches

Awnings and Umbrellas

Awnings add color, forms, relief, shadows, and pedestrian protection from the elements. They may also create a space for identification signage. They should not, however, be used as a substitute for genuine building massing and articulation.

- A. Awnings or signage, should be used to help to clearly demarcate building entries and help orient pedestrians.
- B. Awning maintenance should be in accordance with the awning manufacturer's care instructions. The life of the awning is generally not expected to exceed eight to ten years. Property owners should not propose installing awnings unless they are prepared to replace the awnings every eight to ten years.
- C. Awnings and umbrellas should be made of cloth (not plastic or vinyl) and should be high enough so as not to inhibit pedestrians.
- D. Awnings should not be wrapped around buildings in continuous bands. Awnings should only be placed on top of doors, windows or should be placed within vertical elements when the facade of a building is divided into distinct structural bays.
- E. Awnings should not dominate the facade, but should be in scale with the rest of the building.
- F. Awnings, if lighted, should be lit with direct, architecturally interesting and appropriate fixtures such as goosenecks.
- G. Consider incorporating permanent shading devices such as awnings and canopies on south-facing facades to add aesthetic quality and assist in cooling the building during the summer months.



Awnings and umbrellas add color, forms, and provide solar shading

Exterior Building Materials

The selection and placement of building materials should provide visual interest at the pedestrian level. Heavier materials should be used to form the building base and as accents on upper stories and walls. Architectural details should be used to enhance the buildings and adjacent pedestrian spaces by adding color, shadows, and interesting forms.

- A. Different parts of a building's façade should be articulated by the use of color, arrangement of façade elements, or change in materials to help break up the massing and create an image of a project that has been developed over time.
- B. Details such as wall surfaces constructed with patterns, changes in materials, building pop-outs, columns, and recessed areas should be used to create shadow patterns and depth on the wall surfaces.
- C. Materials should occur at a change in plane where they tend to appear substantial and integral to the structure, preferably at an inside corner.
- D. Material changes not occurring at a change in wall plane appear "tacked-on" and should be avoided.
- E. Materials selected for multi-family residential projects should be very durable and require low maintenance.
- F. Textures, colors, and materials should be unifying elements in the building's design.
- G. Stone, wood timbers, trellises, and other natural materials are encouraged within the Village.
- H. Natural materials such as brick, stone, copper, etc. should be left their natural color.
- I. Materials should be utilized that reduce the transfer of heat into and/or out of the building.
- J. Recycled content and non-toxic materials should be used wherever possible.

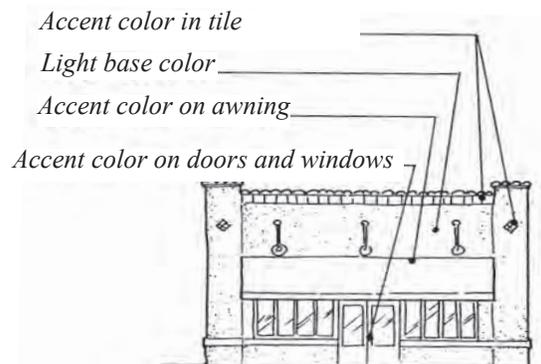
Colors

The following guidelines are intended to promote well-coordinated color palettes that integrate with the other exterior features of a building.

- A. Buildings should keep a balanced palette between colors used on primary wall surfaces and "vibrant" or "darker" accent colors on each building.
- B. Colors should be used that reduce sun glare on wall planes by using flatter, muted colors, i.e. avoiding bright whites.
- C. Door and window trims, awnings, and wall tiles provide opportunity for color that adds interest and texture to storefronts or building bases. Color of trim should be coordinated with the wall colors.
- D. Colors should coordinate with natural unpainted materials used on the facades, such as pressure treated wood, terra cotta, tile, brick, and stone.



A building's appearance can be enhanced with well-coordinated color palettes.



Utilitarian Aspects of the Buildings

Utility service areas should be part of the early building design process, rather than an afterthought at the construction document phase.

Screening of Utilitarian Equipment

- A. Service, utility, and loading areas should be carefully designed, located, and integrated into the site plan. These critical functional elements should not detract from the public viewshed area.
- B. Siting of noise and odor generating functions on any site that may create a nuisance for the adjacent properties should be avoided.
- C. Roof access should be provided from the interior of the building. Exterior roof access ladders should be avoided if possible.
- D. Roof mounted mechanical equipment should be screened from public view.



Utility enclosures should reflect the architectural character of the surrounding buildings



Stone, wood timbers, trellises, and other natural materials are encouraged within the Village

Trash and Recycling Enclosures

- A. Trash and recycling enclosures should be large enough to handle the refuse generated by that site.
- B. A pedestrian entrance to the trash enclosure should be provided so that the large access gates do not have to be opened as often.
- C. Recycling bins should be integrated into the enclosure.
- D. Trash and recycling enclosures should be designed using similar materials and colors as the surrounding buildings within the project so as to reduce their visual impact.
- E. Trash and recycling enclosures and loading areas provided in the parking areas must be screened with landscaping or wall materials, and should be sited in areas where they will be least visible.
- F. Trash and recycling enclosures should be separated from adjacent parking stalls by minimum 3-foot wide planters with low-growing plant materials to ensure that adequate space is available for passengers to access a vehicle in an adjacent parking space.

Trash enclosures and utility enclosures should reflect the architectural character of the surrounding buildings



Disabled-Accessible Ramps and Railing

- A. Ramps and guardrails used as a means of egress must conform to the criteria listed in the Uniform Building Code.
- B. Guardrails should complement the architectural style of the building.
- C. Where possible, ramps should be integrated into the site design to create functional and unique spaces.

Exterior Stairs

- A. Stairways should be constructed of smooth stucco, plaster, or wood, with accent trim of complementary colors. Thin-looking, open metal, prefabricated stairs are discouraged.
- B. Stairways should be designed as an integral part of the overall architecture of the building and should complement its massing and form. Stairwells that appear "tacked-on" are discouraged.

Exterior Utility Panels

- A. Utility service areas, such as electrical panels, should be placed within enclosures that are architecturally integrated into the building design.

Roof Drainage

- A. Gutters and downspouts on the exterior of the building should be decorative or designed to integrate with the building facade.
- B. Roof scuppers should not be used in areas that are visible to the street or public spaces.

Mailboxes

- A. Common mailboxes should be designed similar in form, materials, and color to the surrounding buildings.

Building Signage

A sign program should be submitted with design review applications for new buildings.

- A. Signs reflecting the type of business through design, shape, or graphic form are encouraged.
- B. The method of sign attachment to the building should be integrated into the overall sign design chosen.
- C. Signs should coordinate with the building design, materials, color, size, and placement.
- D. Signs should not cover up windows or important architectural features.
- E. Sign variety is encouraged among different users.
- F. A single development with multiple users should provide a unifying sign theme.
- G. Lighting of all exterior signs should be directional to illuminate the sign without producing glare on pedestrians, autos, or adjacent residential units.
- H. Flush mounted signs should be positioned within architectural features, such as the panel above the storefront on the transom or flanking doorways.
- I. Signs should align with others on the block so as to maintain the existing pattern.
- J. Internally-illuminated sign cabinets are strongly discouraged.
- K. Hanging signs attached to buildings that project perpendicular to the building should be a minimum of 8' from ground level to the bottom of the sign.
- L. Signs should be placed in locations that do not conflict with street or parking lot trees.



Ramps and stairs should be designed as an integrated feature and can provide an opportunity for public art

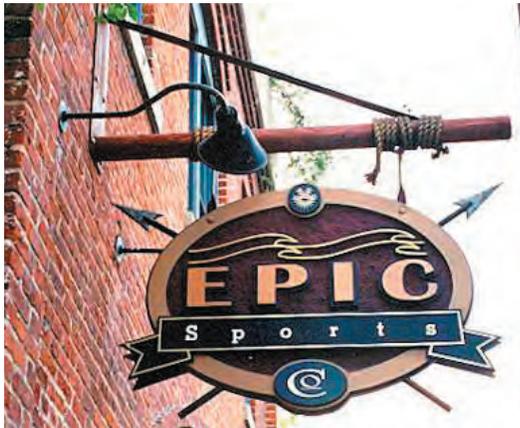


Example of a common mailbox designed to complement the buildings

Monument Signs

Due to the pedestrian orientation of the Village, signage should be limited in scale and quantity.

- A. Monument signs should be well-articulated and well proportioned.
- B. Monument signs should be accented with landscaping. The signs should be in scale with adjacent buildings and landscape areas.
- C. Monument signs should incorporate complementary colors, materials, and lettering fonts used on the buildings. More than one material is recommended.



A sign's design includes the way it is lit and attached to the building



CHAPTER 6: STREET BEAUTIFICATION AND PUBLIC IMPROVEMENTS

Introduction

As the City of Agoura Hills moves forward to revitalize Agoura Village, it is important to understand, recognize, and incorporate by policy elements of good street design. Public streets are truly the public's domain, places where business is conducted, people meet, and where the image of a city is shaped. Streets provide an intrinsic opportunity to shape and add character to our communities. By embracing the street as an important public place, the City can create an environment rather than simply a means to get from point A to point B.

This Chapter of the Specific Plan provides recommendations on street beautification amenities and Village entrances gateways. Figures 6.2 and 6.3 show conceptual streetscape plans for a segment of Agoura Road in Zone A. It is the intent of the city to develop additional "typical" street designs for Kanan Road, Cornell Road, and Roadside Drive. These conceptual plans provide detailed guidance on specific design treatment and materials selections. As all street improvements will be carried out largely by the development community concurrent with new project and redevelopment projects it is critical that these guidelines are used as streetscape improvements.

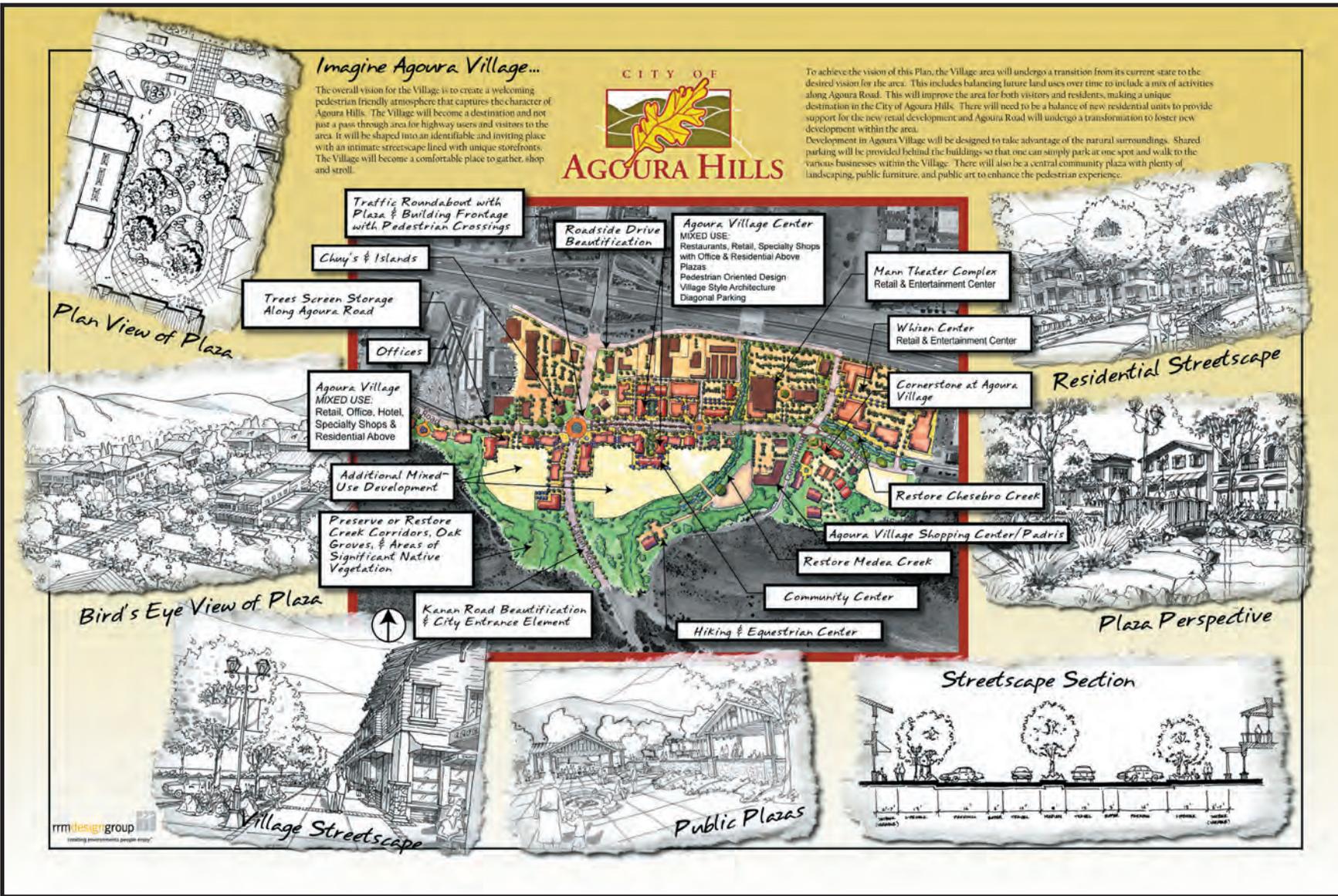


Streetscape beautification is about making our streets comfortable - creating a street scene that is attractive to the eye, un-congested for a social stroll, functional for getting from point to point, and recreational for those wanting leisure. Agoura Village should be a place for residents to walk to a range of uses both day and night. Many elements of the street create its character, from buildings, paving patterns, and furniture to the landscape and storefront activities.



Example of streetscape improvements along Agoura Road

Figure 6.1 - Agoura Village Vision



Village Gateways and Signage

Visual cues, such as signage and markers, create virtual borders and inform pedestrians and drivers they have arrived at a special destination. As a component of the implementation plan, a signage program will be developed for the Village. A strong sign program is one of the fastest ways to make an immediate impact and “brand” an urban area.

- The sign program shall include, but is not limited to, the development of a logo, district monument design, gateway designs, directional signage, advanced warning and street signs, facility identification, directories, and banners.

Village Monuments

Village Monuments should be used to identify primary entrances into the Village area. Their purpose is to signify the boundaries of the area and to enhance the identity of the Village.

- Village Monuments shall be the largest and most detailed signage placed in the Specific Plan area.
- Monuments shall be located at the north and south entrance of the Village on Kanan Road.
- The composition and elements in the District Monuments shall function as a visual anchor.
- Designs shall reflect natural elements and materials found within Agoura Hills and the surrounding area.
- Monuments features shall be significant in scale to clearly identify the area as an important place and shall be easily legible to motorists.

Village Gateways

Village gateway features should be used to identify secondary entrances into the area.

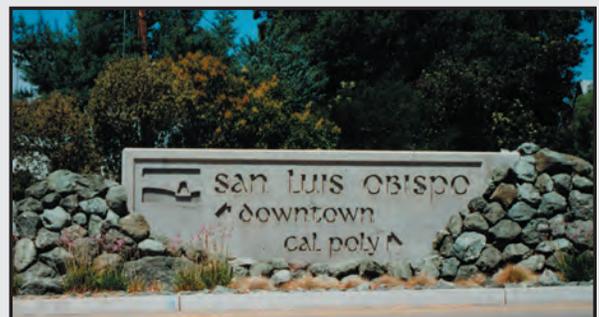
- Gateway monuments shall be located at the east and west entry points along Agoura Road heading into the Village.
- Monuments shall be consistent in materials and colors as the District Monuments located off of Kanan Road.



Example of a decorative banner



Gateway and directional signage



Street Furnishings

Streetscape furnishings are essential elements that are incorporated into the public right-of-way to complement and enhance surrounding development and enhance the pedestrian experience.

Within the Specific Plan area it will be the responsibility of developers to provide and maintain all on-site improvements as well as all streetscape improvements. This chapter defines the streetscape improvements that shall be implemented within the public spaces of the project and at street edge.

The following furniture palette (lighting, benches, waste receptacles, bicycle racks, and tree grates) and wall/hardscape design elements will be used to enhance the Village area.

Lighting

The street and pedestrian lighting concept allows for two separate types of lighting fixtures for the Village area. One lighting standard should be used for the roadways and one standard for the sidewalk areas.

- Lighting, fixtures shall incorporate the latest energy-efficient technology for directing light and reducing glare.
- Consider the following factors to reduce light pollution:
 - (1) The wattage or brightness of the light;
 - (2) The installation of the fixture and use of shields to minimize light spill;
 - (3) The type of light - sodium amber lights are softer than metal halide type; and
 - (4) Placing lights on timers or motion sensors to limit their on-time, as appropriate.

Street lighting

Streetlights shall incorporate a decorative light pole and luminaire that is consistent in design to help unify the streetscape within the Village.

- Streetlighting shall be Boulevard D650 from Sternberg Lighting located pursuant to engineering recommendations.

Pedestrian Lighting

- Pedestrian lighting shall be Sternberg Vintage Lighting - 'Prairie', Model #1230 with square pole.
- These fixtures shall accommodate banner attachment arms, planter arms and electrical outlets.
- Public sidewalk lights shall be approximately 15 feet in height and placed approximately 100 feet apart.



Desired street light fixture for the Village (Sternberg Lighting Model: Boulevard D650)



Desired pedestrian light fixture for the Village (Sternberg Lighting Model: Prairie 1230)

Benches and Trash Receptacles

New benches and trash receptacles shall be installed within the Village area.

- Typical placement of the benches and trash receptacles shall be approximately every 100' to provide seating and amenities for pedestrians.
- Freestanding wood benches shall be Smith & Hawkin - 6' 'Giverny Bench', Model #786442 with natural finish.
- In areas where benches are integrated into landscaped pockets and planters, glass fiber reinforced concrete supports that replicate natural boulders should support wood or concrete benches.
- Refuse receptacles shall be Du Mor Site Furnishings - 'Redwood Receptacle', Model #152-31R with natural finish on redwood slats and bronze powder coat finish on cast-iron rosettes.



Streetscape furnishings

Tree Grates

Although the majority of the streetscape landscaping will be designed in a clustered, free-flowing style, opportunities for trees in tree grates exist in plaza areas and interior courtyards. The installation of tree grates provides room for safer sidewalks, increased opportunities for outdoor benches and seating, and increase the permeable area along sidewalks and can help the health of trees and reduce sidewalk maintenance.

- Tree grates shall be a Neenah Foundry Company - 'Boulevard', 5 foot cast-iron round tree grate with removable light cover & frame, Model #8838-A. Natural Patina Finish.



Tree grate

Planter Pot

Additional landscaping shall be provided in flower pots located adjacent to the building edge.

- Planter pots shall be Wausau Tile - 'Site lines' planter pot. Model #SL400 with 'Black Coffee' patina finish #F1.



Potted plant

Bicycle Racks

Bicycle racks are important amenities that will encourage bicycle ridership in the Village. The 'Inverted U' style rack provides the greatest functionality while being attractive and space efficient.

- Bike racks shall be installed at convenient locations along Agoura Road, at the bus stop, and conveniently located near building entries that are centrally located within development and not facing Agoura Road.
- Bike racks are required as a part of all private development projects in the Village.
- Bike rack shall be Du Mor Site Furnishings - 'Loop Bike Rack', Model #83-00 with Bronze powder coat finish.



Desired Village Bicycle Rack

Paving Materials

Enhanced paving materials improve the pedestrian experience, both in visual appeal and safety. Colored pavers in the street are helpful to raise awareness through increased visibility, noise, and vibration. Their use can often increase the effectiveness of other measures, such as curb extensions and medians.

New concrete sidewalks with a simple scoring pattern should be used along all plan area streets and shall be constructed with a combination of flagstone pavers, exposed aggregate concrete, and scored colored concrete with natural sandstone boulders placed in accent locations (Refer to Figure 6.3).

- The use of structural soils under paving materials is encouraged to increase available rooting area for street trees.
- Flagstone Pavers. Set stones on 4" thick concrete slab and mortar in place.
- Cut Flagstone Paver Banding at tree grates & pedestrian lighting.
- Exposed aggregate concrete. Color concrete integrally with Davis 'Sandstone' color #5237. Select pea gravel material in warm, earthy tones, as shown.
- Scored-colored concrete. Color concrete integrally with Davis 'Sandstone' color #5237. Provide a 4'x4' score pattern.
- Natural sandstone boulders. Stones shall reflect material native to the Santa Monica Mountains. Set boulders on 6" minimum gravel bed allowing 1/3 of boulder height to remain below finish grade.

Bollards

Bollards are often used to delineate between vehicle and pedestrian zones to help create a safe walking environment. Bollards can help define public plazas, expanded sidewalk areas at intersections and walkways in the Village area.

- Bollards shall be placed at plaza and bulbout locations.
- 18"x18"x36" cut sandstone bollard with natural, rough hewn finish. Stone should reflect material native to the Santa Monica Mountains. Core drill & secure bollard to ground with 2" galvanized steel pipe and sleeve. Epoxy in place.

Landscape Recommendations

The landscape design for the Specific Plan area is reflective of the natural setting and creates an identity for the Village. In addition, plants have been selected to provide opportunity for shade, ease of maintenance, and climate compatible planting. The landscaping will establish a visual integrity for the area, and promote pedestrian and vehicular safety by clearly distinguishing walkways and access points.

The following landscaping palette shall be used throughout the Specific Plan area (Refer to Figure 6.2).

Street Trees

Street trees are large trees generally selected for their ability to form dense, round canopies, grow well under paving areas and produce limited litter. The following is a list of street trees to select from.

- Pistacia chinensis, Chinese pistache
- Platanus acerifolia, London Plane Tree
- Quercus agrifolia, Coast Live Oak
- Platanus racemosa, California Sycamore
- These trees shall be spaced unevenly in order to reinforce a natural feeling within the landscape.
- Street trees shall be carefully located to not obstruct visibility of the store fronts and signage nor negatively impact pedestrian and vehicular safety.
- All street trees should be installed with electrical outlets for holiday lighting.
- Due to the utility poles along Roadside Drive, not all of these species may be appropriate. Therefore, along Roadside Drive, selection of street trees shall be made in consultation with and after approval by the City's Landscape Consultant.



Sandstone Bollards



Exposed aggregate concrete



Flagstone textured paving accents shall be used



Natural sandstone boulders

Accent Trees

Accent trees are generally smaller than standard street trees and are used to provide special treatments to key locations such as intersections, pedestrian crossings and public gathering spaces.

Accent trees are selected for the foliage color or flower showing but close attention is also given to their ability to perform well under various paving types. The following is a list of accent trees.

- *Cercis occidentalis*, California Redbud
- *Lagerstroemia indica*, Crepe Myrtle
- All accent trees shall be installed in tree grates with up-lighting.



California Sycamore

Shrubs and Groundcovers

The shrubs and groundcovers below have been selected to reinforce a natural feeling within the landscape. The following palette is generally comprised of drought tolerant, predominantly native plant species.

- *Agave americana*, Century Plant
- *Arctostaphylos* Howard McMinn, Howard McMinn manzanita
- *Cistus purpureus*, Purple Rockrose
- *Convolvulus cneorum*, Bush Morning Glory
- *Convolvulus sabatius*, Ground Morning Glory
- *Festuca ovina glauca*, Blue Fescue
- *Helictotrichon sempervirens*, Blue Oat Grass
- *Hemerocallis* hybrids, Daylily hybrids
- *Perovskia atriplicifolia*, Russian Sage



Coast Live Oak

- *Phormium tenax rubrum*, Coastal Flax
- *Rosmarinus officinalis*, Rosemary
- *Salvia leucantha*, Mexican Bush Sage
- *Trachelospermum jasminoides*, Confederate Jasmine
- *Tulbaghia violacea*, Society Garlic

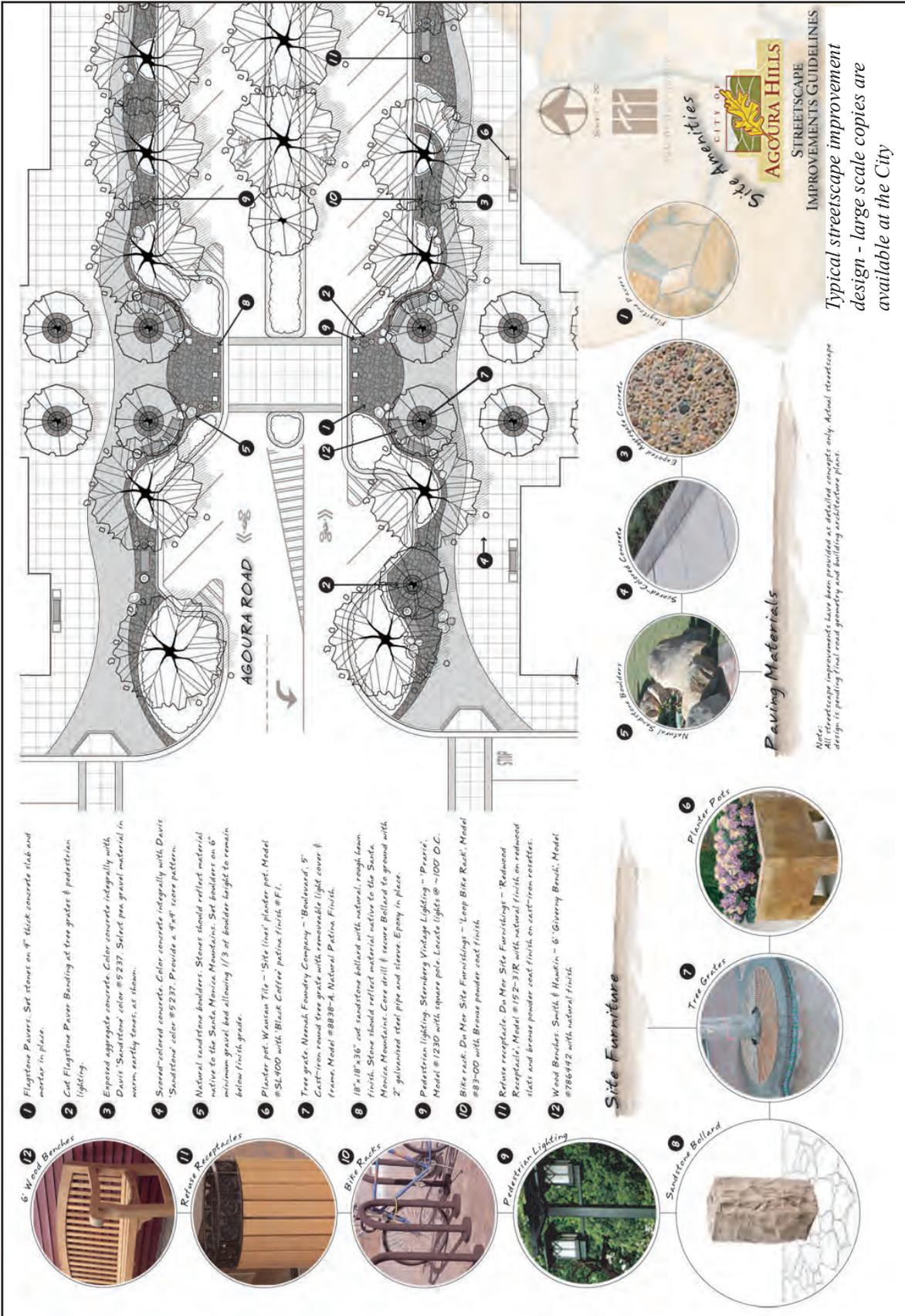


Figure 6.3 - Streetscape Improvement Furnishings and Paving Guidelines

CHAPTER 7: INFRASTRUCTURE AND PUBLIC SERVICES

This chapter addresses issues concerning water, waste water, solid waste, public safety, and educational facilities within the Agoura Village Specific Plan area. This analysis is based on the projected full build-out of new development in the Agoura Village Specific Plan area, which would include a maximum of 293 dwelling units and up to 575,958 square feet of new commercial and retail development in addition to the existing 372,042 square feet of development currently located on the site.

Public Services

Water Supply

Currently, the City of Agoura Hills is serviced by the Las Virgenes Municipal Water District (LVMWD), which supplies potable water to the City. The potable water supply for the proposed development would be delivered by the LVMWD water system, which currently services existing development within the Specific Plan area. The City's projected water supply is adequate to serve the expected demand from incremental new development within the project area. Some new uses may demand greater amounts of water for their operations. For example, restaurants typically demand more water than retail shops. Likewise, residential uses typically demand more water than office space.

- All new water line extensions or line size modifications shall be designed in accordance with applicable provisions of the Municipal Code and to the satisfaction of the City Engineer.
- As part of development review, the Public Works Department shall determine whether projects are required to submit water pressure and flow demand calculations to provide information to determine if adequate line capacity exists.



Wastewater

The closest wastewater facility to the project area is located five to six miles to the south, and is operated by the Las Virgenes Municipal Water District (LVMWD), which provides sewer service to most of the City. All wastewater generated within the City of Agoura Hills area is transported to the Tapia Water Reclamation Facility for treatment.

- All new sewer line extensions, or line size modifications, shall be designed in accordance with applicable provisions of the Municipal Code and to the satisfaction of the City Engineer.
- As a part of the development process, and prior to project approval, the Public Works Department may require development projects to submit a sewer study to provide information to determine if adequate line capacity exists and to project future flow volume and remainder capacities in the downstream sewer segments.

Solid Waste

Under the City of Agoura Hills' waste hauler franchise agreement, private waste haulers collect, haul and dispose of solid waste in the City. Waste from the City is transported to several landfills throughout the Southern California area. In an attempt to reduce solid waste landfill disposal quantities, the City currently implements a curbside recycling program and a curbside green waste recycling program for residential and non-residential uses.

- Development projects, both during construction and operationally in the long term, shall comply with all City recycling requirements.

Utilities

Gas and electricity utility services are provided to the Specific Plan area by Southern California Gas and Edison, respectively. Cable services are provided by two companies: Adelphia Cable and Charter Cable. Telephone services are provided by SBC.

- As developments are proposed, applicants should consult with utility service providers to determine locations and adequacy of service connections.

Public Safety

The incremental growth anticipated by the Agoura Village Specific Plan will impact public safety services from both police and fire departments. This would include primarily additional personnel, sworn officers, and firefighters to serve this additional population. Existing facilities and proposed equipment will serve this buildout of the Specific Plan.

Police Protection

The City of Agoura Hills contracts for police protection service with the Los Angeles County Sheriff's Department, which currently provides police services for the entire Agoura Hills area. The primary station that services the City is the Malibu/Lost Hills station in western Los Angeles County.

In addition, the station participates in a reciprocal agreement with stations in the nearby communities of Westlake and Calabasas, which enables these stations to be called upon for assistance, if necessary.

Fire Protection

The City of Agoura Hills contracts with the Los Angeles County Fire Department for fire protection services. Specifically, Station No. 65, located along Cornell Road south of the City limits, would provide fire protection and emergency medical services for the AVSAP project area. The construction of Fire Station 89, located at 29575 Canwood Street was recently completed. All development projects are subject to payment of applicable developers fees for fire protection.

Educational facilities

The Las Virgenes Unified School District (LVUSD) provides educational services within the City of Agoura Hills. The proposed Plan area is located within the service area of Agoura High School, Lindero Canyon Middle School, A. E. Wright Middle School, Yerba Buena Elementary School, Willow Elementary School, and Sumac Elementary School. There are no school sites located within the Plan area. Schools located within the City of Agoura Hills are as follows:

Agoura High School
28545 West Driver Avenue
Agoura Hills, CA 91301
Enrollment: 2,159
Capacity: 1,900

Lindero Canyon Middle School
5844 Larboard Lane
Agoura Hills CA 91301
Enrollment: 1,170
Capacity: 1,300

Sumac Elementary
6050 North Calmfield Ave.
Agoura Hills, California 91301
Enrollment: 593
Capacity: 598

Yerba Buena Elementary School
5844 Larboard Lane
Agoura Hills, California
Enrollment: 459
Capacity: 600

Willow Elementary School
29026 Laro Drive
Agoura Hills, California 91301
Enrollment: 578
Capacity: 625

All proposed developments shall pay school impact fees pursuant to State law and as adopted by the Las Virgenes Unified School District.

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CHAPTER 8: PLAN IMPLEMENTATION

Introduction

The Specific Plan Implementation chapter is meant to serve as an action plan to guide the City in initiating many of the recommendations made by this Specific Plan. It is the intent of the City that a partnership be formed between the public and private sectors to carry out all of the initiatives of the Specific Plan.

The contents of this Chapter include:

- A summary description of Specific Plan recommendations including capital projects and programs;
- A summary of potential funding sources that may be available to the City for implementation of various Specific Plan recommendations;
- A summary of potential financing opportunities and;
- A summary of potential business recruitment and retention strategies.

One of the first steps in the Specific Plan preparation process was to conduct a market feasibility study. The study was prepared by Stanley R. Hoffman and Associates and provides important economic and market data served as a basis for developing the Specific Plan, especially the land use policies and development standards. A summary of this study is provided in Chapter 2, Existing Conditions.



Summary of Specific Plan Recommendations

Capital projects

The Agoura Village Specific Plan identifies numerous capital improvements to be made within the public right-of-way. These improvements need to be fully coordinated with future private development and the City's overall capital improvement program to ensure that streetscape amenities work with infrastructure requirements.

Circulation facilities:

1. Roads and Streetscape improvements

- a. Agoura Road, Kanan Road, Roadside Drive and Cornell Road: Per the recommendations of the Specific Plan (see Chapters 3 & 6), street reconfiguring, lane geometry and re-striping for vehicles and bicycles, lane transitions, transit stop and bus shelter, curb and street engineering modifications, drainage systems, utilities, landscaping and irrigation improvements are necessary. In addition, streetscape improvements include such elements as sidewalk paving, furnishings, lighting, other amenities as specified by the Specific Plan, and, if applicable, medians, mid-block crossings, roundabouts, etc. All street improvements listed above shall be constructed concurrent with adjoining private development.

2. Kanan/Agoura Roundabout

- a. Per Specific Plan recommendations and conceptual design (see Chapter 3), a traffic roundabout at the intersection of Agoura and Kanan Roads. Conceptual plans have been prepared by Orston-Roundabout Engineering and are on file with the City Public Works Department. Roundabout improvements shall be constructed concurrent with the first private development project adjoining the roundabout. The City shall assist in preparing and administering a reimbursement agreement between area developers.

3. Trails and Paths

- a. Equestrian path along Medea Creek. The City shall take the lead in the design and engineering, path grading, surface materials, fencing, signs, landscaping and irrigation systems to fully design and construct the facility. The path shall be located between Agoura Road and the proposed equestrian center on Cornell Road as shown in the Specific Plan (Refer to Chapter 3, Mobility.)
- b. Pedestrian trail along Chesebro Creek: The City shall take the lead in the design and engineering, fencing, signs, landscaping and irrigation systems. The trail shall be located adjacent to Agoura Road east of Cornell Road to the plan area boundary where it will ultimately connect with an extension of the trail further east under a separate project. (Refer to Chapter 3, Mobility.)
- c. Pedestrian trails along Lindero Canyon and Medea Creeks. Pedestrian trails and associated creek buffering and revegetation along these two creeks shall be undertaken by private development and constructed concurrent with adjacent private development in Zones A and B. (Refer to Chapter 3, Mobility.)
- d. Pedestrian bridge: Concurrent with public parking established at the County of Los Angeles as a result of the parking study, the City shall take the lead in the design and construction of a pedestrian bridge over Medea Creek connecting the public parking lot with Zone A South development. Funding for this improvement shall be provided by new development.

Village identity

1. Logo: City to administer preparation of a logo for use in all identity and branding in the Specific Plan area. This logo will be used at entrance monuments, street signs, directional signage, gateways, banners and other branding materials.
2. Gateway Monuments: City to administer the design and construction of gateways monuments as recommended by the Specific Plan. Funding for these features via potential funding sources outlined in this chapter.
3. Banners: City to administer the design and fabrication of banners as recommended by the Specific Plan. Funding for these features via potential funding sources outlined in this chapter.

Programs, studies and initiatives

1. Incompatible Land Uses: Identify those properties that inherently conflict with the uses envisioned in the Agoura Village Specific Plan and seek cooperative resolution. Explore opportunities for owner participation, property rehabilitation, cooperative acquisition and compliant redevelopment. The Specific Plan provides specific regulations which allow exceptions to the FAR and development standards to encourage short-term redevelopment of the Agoura Road frontage of these properties. (Refer to Chapter 4, Land Use and Development Standards.)
2. Create a conducive development environment: The City shall assume a proactive/assertive posture, actively engaging in capital improvement programming for Agoura Village, guidance and design review for development applications and supporting real estate development activities that are consistent with the Specific Plan. The City can provide support for private development of a mix of uses through incentives, such as timely processing, flexibility to development standards, and/or possible fee adjustments.

3. Prepare a comprehensive parking management and facility study: The purpose of this study is to outline methods to effectively use all existing parking within the Specific Plan area as a priority to constructing new public parking facilities, either surface lots or structures. This study should include the following:
 - a. Analyze existing parking conditions within the Specific Plan area; public parking, private parking, on and off street, quantities, locations, peak hours of use, duration of occupancy of spaces, and other parking attributes.
 - b. Forecast projected parking demand at Specific Plan build out and identify on- and off-street quantity and locations, peak hours of use, duration of parking, priority zones within Specific Plan area for additional public parking, shared parking opportunities, and others.
 - c. Prepare plans and negotiations to acquire land and construct facilities for public parking pursuant to findings determined in the study.
4. Analyze potential for creation of parking district utilizing in-lieu fees or parking assessment for the purpose of funding public parking.
5. Transit service: Study transit service enhancements to Agoura Village through the preparation of a transit study. Study should consider establishment of an Agoura Village trolley with loop service to destinations south of Highway 101 and immediately north of Highway 101 with the intent of providing alternative transportation services to the Village area.
6. Community center and sheriff substation: City to work with developers of Zone A south to generate a preliminary facility program and conceptual design, to affirm the range of users and space utilization, quantify building and site requirements, determine infrastructure needs, define precise site location within development area, review and guide design and development plans.
7. Public Plaza: City to work with developers of Zone A south to generate a preliminary facility program, conceptual design, quantify site requirements, determine infrastructure needs, define precise site location within development area, review and guide design and development plans.
8. Trail connection under freeway: Prepare a trail feasibility study for a new creek side trail between Agoura Road and Canwood Street on the north side of Highway 101. The study shall be undertaken to determine the potential alignment, design, costs, and timing.
9. Storefront and Façade Improvement Program: Prepare a new storefront and façade improvement program for the Specific Plan area. Fund storefront and façade improvement projects through provision of grants in compliance with adopted program
10. Equestrian Center: A feasibility study for a new Equestrian Center as recommended in the Specific Plan, which shall be located adjacent to Cornell Road, shall be prepared. The study should include; needs assessment, programming, conceptual design, capital and operational cost analysis, facilities management and the development and ownership structure.
11. Sign and Way-finding program: Prepare a way-finding directional sign program for the Specific Plan area. Program shall include incorporation of City logo or other Agoura Village identity brand, informational and directional sign designs to facilities such as public parking, public facilities and other important destinations. The program should include sign hierarchy, conceptual designs and shall be prepared with community involvement and be consistent with the guidelines and recommendations of the Specific Plan (Refer to Chapter 6, Street Beautification and Public Improvements.)
12. Public Space: Establish an in-lieu fee program for public space requirements for the City to acquire land and develop open space elsewhere within the Village.

Policy Initiatives	Timing	Reviewing Agency
Capital Projects:		
Roads and streetscape improvement	Concurrent with development	€
Kanan/Agoura roundabout	Concurrent with development	€
Equestrian path along Medea Creek	Long-range	€
Pedestrian trail along Chesebro Creek	Long-range	CD/€
Pedestrian trails along Lindero & Medea Creek	Concurrent with development	CD/€
Pedestrian bridge over Medea Creek	As recommended by parking study	CD/€
Logo	Short-range	CD/€
Gateway monuments	Short-range	CD/€
Banners	Short-range	CD/€
Programs, studies and initiatives:		
Incompatible land uses	On-going	RA/CD
Create a conducive development environment	On-going	RA/CD
Prepare a comprehensive parking management and facility study	Long-range	CD/€
Prepare transit service study	Long-range	CD/€
Community center	Concurrent with development in Zone A South	CD
Public plaza	Concurrent with development in Zone A South	CD
Trail connection under freeway	Concurrent with development in Zone A South	CD/€
Storefront and façade improvement program	Short-range	CD
Equestrian center	Long-range	CD/€
Sign and wayfinding program	Short-range	CD/€
<p>Reviewing Agencies:</p> <p>CD = Community Development</p> <p>RA = Redevelopment Agency</p> <p>€ = Engineering Department</p> <p>Timing:</p> <p>Short-range = 0 to 5 years</p> <p>Long-range = 5 to 10 years</p>		

Table 8.1 - Implementation Strategies

Figure 8.1 - Public Improvements



Potential Funding Sources

This section describes potential funding sources that could be used for implementing the improvements as well as supporting annual operations and maintenance. The sources identified are intended for consideration and have not specifically been authorized at this time.

Arterial Street System Development Fees (traffic improvement fees)

The City currently assesses traffic improvement fees for the construction of circulation improvements to its major thoroughfares. These fees can be used to fund public improvement projects. The arterial street system development fees have already been allocated for other uses in the City's Capital Improvement Program. The City Council would have to reprioritize the listing that already exists for future use of these fees. The existing fees are applied as follows:

This fee applies to residential and non-residential land uses. Commercial projects are assessed a base impact fee of \$2,440 per peak hour trip, which is then adjusted depending on the number of trips generated by the particular land use/development type. This results in a fee ranging from \$4,183 per 1,000 gross square feet for a business park, to \$9,760 per 1,000 gross square feet for a shopping center. New residential projects are assessed the impact fee of \$2,440 times the number of new multi-family units to be constructed.

Quimby fees

The "Quimby fee" provision of the Subdivision Map Act permits the City to require that developers either dedicate parkland or pay an equivalent fee that allows the City to buy land for parks. The fee applies to residential subdivisions and is based upon the number of units multiplied by a density factor. This fee must be used to purchase land for parks in order to meet the City's standards of 3 acres of parkland per 1,000 population.

Capital Improvement Program

As identified in this Specific Plan, a number of public enhancements, services and infrastructure improvements are recommended. Coordinating these improvements and services should be incorporated into the City's Capital Improvement Program (CIP) that also integrates other priority projects and funding sources throughout the City. This will provide a blueprint for successful implementation of Agoura Village in the context of the entire City. The City's Capital Improvement Program is currently allocated to street improvements that are funded primarily by gas taxes and traffic improvement fees.

Business Improvement Districts (BID)

In California, there are two different types of business improvement districts, one created through assessments on business licenses within the district (including landowners who lease property), and the second created through assessments of property owners alone.

Business-based BIDs

The Parking and Business Improvement Area Law of 1989 provides the legal basis for what is commonly known as business improvement districts (BIDs). Business improvement districts utilize an annual assessment levied against business owners based on a measure of benefit to each participating business, and can support a limited range of ongoing activities, including streetscape and sidewalk improvements, trash and cleanup activities, promotion and advertising and public safety. Special assessments within this type of business improvement district cannot be used to secure loans or bonds, thus limiting financing strategies to a pay-as-you-go basis.

Assessments must be directly proportional to the estimated benefit to be received and are levied on businesses in a specified area. Generally, the assessments can be used to finance benefits such as:

- Fountains, benches and trash receptacles
- Street lighting

- Decorations and public art
- Promotions of public events benefiting area
- Furnishing music to any public place in the area
- Promotion of tourism within the area (only businesses benefiting from tourist visits can be assessed for this type of benefit)
- Any other activities that benefit businesses located in the area

Property-based BIDs

The Property and Business Improvement District Law of 1994 provides funding for a wide range of improvements and activities. Assessments are levied against property owners rather than businesses - a key difference from the business based Parking and Business Improvement District Law of 1989, as previously described. Property owners are often more willing to assess themselves if the result may directly benefit their investment in real property. Any assessments must be reauthorized by a majority vote in proportion to their assessment every five years. Compared with a business-based business improvement district that requires annual reinstatement, this longer five-year period also allows for minor capital improvements to be funded through loans.

Assessments must be directly proportional to the estimated benefit to be received and are levied on property owners in a specified area. The assessments can be used to finance benefits listed above as well as benefits such as:

- Closing, opening widening or narrowing existing streets
- Rehabilitating or removing existing structures, facilities or equipment to enhance security within the area
- Marketing and economic development
- Security, sanitation, graffiti removal, street cleaning and other municipal services supplemental to those normally provided by the municipality.

Redevelopment Agency - Affordable Housing

Agoura Village is located within the boundaries of the City's Redevelopment Project Area. State redevelopment law requires that the redevelopment agency set aside 20 percent of any revenues generated by use of property tax increments for low- and moderate-income housing needs. State redevelopment law requires that all redevelopment agencies maintain a 5-year implementation plan, including a 10-year plan for housing production. The Agoura Village Specific Plan area provides opportunities for production of affordable housing, both for rental and ownership, by very low, low and moderate income households through use of the 20% housing set aside fund. Non-housing funds will be used to make pass-thru payments to other taxing agencies, RDA operational costs, fund the Kanan & Reyes Adobe Interchange projects, and address other infrastructure deficiencies.

Federal Sources

Federal funds, such as the Safe, Accountable, Flexible and Efficient Transportation Equity Act: A Legacy for Users (SAFETEA-LU) and Surface Transportation Program - Local (STPL) or other grants would be sought after for infrastructure improvements.

State Sources

State funds such as AB 2928 beautification grants, or other state sources could be sought after.

City Funds

City Funds can be sought such as Traffic Safety funds, General funds, Reserve Funds CDBG, etc.

Regional Sources

The Transit Development Program is funded by two half-cent sales tax measures approved by Los Angeles County voters to finance a Transit Development Program. Twenty-five percent of the Proposition A tax and twenty percent of the

Proposition C tax is designated for the Local Return (LR) Program, which provides funds to cities and the County to develop and/or improve public transit, paratransit and the related transportation infrastructure. LR funds are allocated and distributed monthly, on a per capita basis by the MTA. The City of Agoura Hills currently uses these funds for street maintenance and improvements.

Proposition A Local Return Funds

These funds must be used exclusively to benefit public transit. Eligible uses of these funds include expenditures related to: fixed route and paratransit services, Transportation Demand Management, Transportation Systems Management and fare subsidy programs that benefit transit exclusively. These funds can be traded among jurisdictions in exchange for general or other available funds.

Proposition C Local Return Funds

These funds must be used exclusively to benefit public transit but can be applied to a wider list of eligible project expenditures, including Congestion Management Programs, bikeways and bike lanes, street improvements supporting public transit service and Pavement Management system projects. These funds cannot be traded.

Donor Programs

Some of the proposed improvements may lend themselves to a public campaign for donor gifts. Donor programs have been used very successfully in many cities in the United States for providing funds for streetscape and community design elements. Such programs can be tailored to solicit contributions from individuals, corporations, local businesses and community and business associations. Many improvements could be funded by donor gifts for items such as: benches, trash receptacles, street trees, street tree grates, public art elements and information kiosks. Donors could be acknowledged with a plaque on the element itself or other prominent display such as a "wall of fame" with donors' names.

Financing of Public Improvements

As with any new development or redevelopment project, the Agoura Village project developers shall be responsible for making all frontage and street improvements as required by this Specific Plan.

The City may allow credits against required Traffic Impact Fees (TIF) for the construction of street improvements for Agoura and Kanan Roads.

The overall strategy is that those improvements or services that are directly attributable to the newly developing properties will be privately financed, while those improvements that have broader public benefit will be financed through the City and/or other outside sources.

The Specific Plan provides a density bonus program which would allow new and redevelopment projects to earn an increase in residential density in exchange for an extraordinary contribution to desired public facilities. (Refer to Chapter 9) this is a potential funding program for the facilities recommended by the Specific Plan.

The City will take the lead in implementing the following public improvement projects:

- Gateway monuments
- Logo and sign program
- Equestrian paths
- Public parking lot as Los Angeles County site
- Pedestrian bridge at Los Angeles County site

Funding for Operations and Maintenance

Operations and maintenance costs become the joint responsibility of the benefiting private property owners and businesses as well as the City government. Annual maintenance costs for existing lighting, signage, striping and other street-related maintenance items are included in the existing Public Works budget.

- A. A Landscape Maintenance District (LMD) could be established to assess benefiting property owners and cover the maintenance costs to the City of servicing landscaping and street lighting, rather than supporting maintenance through the City's General Fund. Each parcel is assessed a portion of the costs of the services to be financed by the LMD, based on the proportion of benefit received by that parcel
- B. Each property owner could be responsible for the maintenance of the site frontage.
- C. A Business Improvement District (BID) could be established to utilize a special assessment levied against business or property owners based on a measure of benefit to each participating business. This could support a range of ongoing activities, including streetscape and sidewalk improvements, trash and cleanup activities, promotion and advertising and public safety.
- D. A voluntary property owner's association could be formed, whereby funds are accumulated similar to a Business Improvement District (BID). However, it is more informal than a BID, and fees are not collected as part of the property tax bill.

Potential Business Recruitment Strategies

This section presents a plan for attracting businesses to Agoura Village. The locational advantages of the area, including its prime access off U.S. 101 and the well-traveled arterial Kanan Road, give the Agoura Village area the potential to become a vibrant town center. However, attracting the appropriate tenant mix to the project area is key in order for Agoura Village to establish a competitive position with centers in nearby communities.

The purpose of a business attraction program is to provide a strategy for establishing a desirable mix of destination and complementary retail uses in Agoura Village. The program will include goals to support the vision of Agoura Village, strategies and actions for achieving these goals, and will identify the key demographics and competitive setting of the market area. An action plan should utilize successful strategies from other shopping districts that may be appropriate for Agoura Village.

Proposed goals for Agoura Village include the following:

- Foster the economic growth and health of the Village through a strong retail recruitment and retention program.
- Create a unique retail concept and tenant mix to distinguish Agoura Village from other nearby shopping/dining/entertainment centers.
- Create an action plan that builds upon existing strengths and provides maximum public benefits from limited public resources.
- Attract an array of businesses that are within the competitive market and demographic opportunities and constraints defined in the Market Feasibility Study.
- Assist in the implementation of a program aimed at actively involving property owners, landlords, existing tenants, commercial brokers, developers and City staff.

Elements of Retail Retention and Business Attraction

Once the goals have been defined, directed action can take place. A successful program will focus on business retention first to make sure that the existing retail base is strong. After this, retail recruitment activities can take place. A proposed outline for a business attraction plan should include the following components, building on the existing Market Feasibility Study:

- Definition of the Market Area
- Business Attraction Strategy
- Business Retention
- Recruitment Activity

Business Attraction Strategies

The following are possible business attraction strategies:

- A. Determine the names of target local, regional and national tenant types. Determine the candidate's retailers' square footage and locational requirements and their potential interest in locating within Agoura Village.
- B. Prepare marketing materials that promote Agoura Village as an attractive place for conducting business. Marketing materials must include several basic items:
 1. A general description of the Agoura Village concept should be prepared with a strong marketing orientation, including property types, site maps and plans.
 2. Basic facts about the retail marketplace should be provided including:
 - Demographics
 - Competitive retailers located in the market area and brief descriptions of the area's retail centers
 - Retail voids that may exist in various retail categories within the retail trade area of the Village

- Current range of lease rates, vacancy rates, and sales volumes
 - Incentives
 - Parking
3. Renderings or sketches of planned individual developments and of the public space, and photos of the existing retail businesses.
 4. Potential floor plans for retail spaces should be prepared by property owners for inclusion in the marketing materials. If floor plans are not available at this point, then some description of build-to-suit buildings and individual retail spaces should be developed and included in the marketing materials. The potentials for “build-to-suit” to make a building or retail space suit a tenant’s specific needs should be emphasized.
 5. Time schedules for phasing of public improvements and the development of the entire Village.
 6. Names and contact phone, fax, and e-mails of key City staff of the various City departments involved in the development approval process
 7. Means for contacting property owners who own property to be developed in the Village
 8. Marketing materials should include color brochures and mailing flyers, videos and information on the Agoura Hills City web site.
- C. Retention and recruitment of retailers could be conducted through a variety of tactics:
1. Working with the existing property owners in the Village to augment their efforts at recruiting and leasing.
 2. Distribution of marketing information to real estate brokers, both those serving the Agoura Hills area and those representing desired retailers.

3. Appearances, booths, etc, at major professional conferences, particularly those of the International Council of Shopping Centers (ICSC) and the Urban Land Institute (ULI).
4. Mailings of marketing materials to desired retailers.

CHAPTER 9: PLAN ADMINISTRATION

This section describes the relationship to other planning policy and documents, and the administrative procedures required for amendments and/or modifications to the Specific Plan, as well as unique review requirements for development applications within the Specific Plan area.

Specific Plan Adoption

The Specific Plan was adopted by City Council Resolution. Adoption of this Specific Plan involved City Council approval of a General Plan Amendment (text and map), a Zoning Ordinance and Map Amendment, and Amendment to the Ladyface Mountain Specific Plan. Upon adoption, the Specific Plan and Environmental Impact Report established the land use and supplemental development standards for the Specific Plan area.

Relationship to Other Planning Documents

Described below is the relationship of the Specific Plan to other existing regulatory documents.

- Ladyface Mountain Specific Plan: Where this Plan overlaps with the Ladyface Mountain Specific Plan, the Agoura Village Specific Plan supersedes the Ladyface Mountain Specific Plan. Zones B, F, and G of the Agoura Village Specific Plan (SWC of Kanan and Agoura) overlap with the Ladyface Mountain Specific Plan. The Ladyface Mountain Specific Plan will be amended to delete those territories from the Ladyface Mountain Specific Plan such that only the Agoura Village Specific Plan applies.
- Hillside and Significant Ecological Area provisions and the Grading Ordinance: All provisions apply except for Section 9652.13 A and B of the City's Hillside and Significant Ecological Area provisions.
- Freeway Corridor Overlay District: Portions of the Agoura Village Specific Plan overlap the Freeway Corridor Overlay District. The Zoning



Ordinance and Map will be amended to delete the Freeway Corridor Overlay from all properties in Agoura Village.

- Agoura Village Overlay: The Agoura Village Overlay District is superseded by the Agoura Village Specific Plan in all respects and will be removed from the City of Agoura Hills Municipal Code.
- Open Space Zoning: Any property located in the AVSP that has a land use designation of Restricted Open Space (OS-R) or Restricted Open Space Deed Restricted (OS-R/DR) at the time of adoption of the Specific Plan shall be subject to Ordinance No. 99-300. Ordinance No. 99-300 requires a 2/3 vote of residents to utilize the property for any use other than those uses listed in the Open Space Zone in the Zoning Ordinance.

Specific Plan Administration

The Agoura Hills Planning and Community Development Department is responsible for the administration, implementation, and enforcement of this Specific Plan.

All projects involving new construction must follow a three step process. The steps are as follows: (1) Concept Review, (2) Preapplication Review and (3) Submittal of a Formal Application. The Concept and Preapplication Reviews are mandatory and no formal application will be accepted until the landowner/developer has gone through both of these processes. The Concept Review should be undertaken very early in the process, while the actual site layout is in the preliminary stage.

The primary objective is to ensure that new projects are designed consistent with the Agoura Village vision early in the review process. This will save landowners and developers time and money and help projects that adhere to the vision in the Specific Plan move more smoothly through the approval process.

The Concept Review will focus on site planning principles rather than on the architecture of the buildings. The review will focus on the creation

of blocks and streets, placement of buildings, location of parking, building types, design of the public realm and pedestrian and vehicular linkage between other projects planned or existing in the area. The Preapplication Review will consist of a review of the buildings and architecture that will occupy the blocks and activate the streetscape and open space. Details of the review process, including submittal requirements, are available at the Planning counter at City Hall. Upon completion of the Concept Review and Preapplication Review process, a formal application for an Agoura Village Development Permit may be submitted to the City.

Agoura Village Development Permit

Any new development or renovation of existing property shall require an Agoura Village Development Permit (AVDP). An application for an AVDP shall be processed in the same manner as an application for a Conditional Use Permit pursuant to Division 3 Conditional Use Permit in the Zoning Ordinance.

The Agoura Village Development Permit is designed and intended to provide for the orderly development of land in conformance with the vision contemplated by the Agoura Village Specific Plan. An AVDP allows for innovations and flexibility in site development, including the location of structures, conservation of natural land features, and efficient utilization of open space. The permit would allow a flexible design approach in order to establish an environment equal to or better than that resulting from the application of the minimum standards of the Zoning Ordinance. Through the AVDP process, project design review will be conducted to ensure that projects exceed or meet the development standards and design guidelines contained in the Specific Plan.

Findings

Approval of an Agoura Village Development Permit shall require that the Planning Commission make all the following findings:

- The proposed project complies with relevant provisions of the adopted Specific Plan.

- The proposed project incorporates design measures to ensure maximum compatibility with the vision of the Agoura Village Specific Plan.
- The proposed project incorporates architectural and landscaping elements that enhance the area.
- The project's vehicular access and parking plan minimizes conflicts and promotes efficient internal circulation and shared use of facilities wherever feasible.

Agoura Village Specific Plan Standards

All development must comply with the standards and guidelines established in the Agoura Village Specific Plan except as noted below:

Height

The City Council on recommendation from the Planning Commission may approve exceptions to the maximum building heights established by this Specific Plan through the AVDP process subject to the following findings:

- The additional height makes a positive contribution to the overall character of the area and will be compatible with its surroundings.
- The additional height will not result in unreasonable restrictions of light and air to adjacent properties or the public right-of-way, or otherwise be detrimental to the public health, safety and welfare.
- The additional height will support other policies and standards of the Specific Plan and will result in a better overall project.
- The building includes horizontal and vertical step backs to achieve increased articulation and compatibility.
- The form, mass, and profile of the individual buildings and architectural features are designed to blend with the natural terrain and preserve the character and profile of any natural slope or hillside.

In no case would a building be allowed to exceed three stories.

Off-street Parking

All projects must provide off-street parking as required by the Agoura Hills Zoning Ordinance. However, as part of the AVDP application process, all projects are also subject to preparation and submittal of a parking study to assess projected demand and potential for parking reductions in light of mixed uses, non-conflicting peak hour parking demands, and shared parking with adjacent sites. The parking study must also address location and site design of proposed parking lots and/or planning facilities. The Planning Commission, as part of its review of the AVDP, will take the parking study into consideration in determining the necessary off-street parking for the site. Based on the results of the parking study, the Planning Commission shall have the ability to reduce or increase the number of parking spaces provided.

Shared Parking

When two or more land uses or uses on the same site have distinctly different hours of operation (e.g. theater versus office) subject to the review and approval of the AVDP by the Planning Commission, such uses may develop shared parking agreements to satisfy the parking requirements of this Plan in accordance with the following:

- Only 50 percent of the required parking may qualify for shared parking arrangement.
- A minimum of 50 percent of the required parking must be met on-site notwithstanding the parking reduction provisions of this plan.
- Required parking must be calculated based on the land use that demands the greatest amount of parking.
- The shared parking facility must be within a 700 foot radius of the subject use.

If shared parking spaces are located on a different lot, approved off-site parking spaces shall be obtained by a covenant, lease, bond, or other agreement, acceptable to the City Attorney, between the owners, and if applicable the owners and lessees of the off-site parking spaces.

Mixed Use Parking

When a project contains a vertical mix of uses composed of retail commercial or office uses with residential and/or office use above in the same building, the non-residential portion of the mixed use building may be eligible to receive a reduction in the parking requirements established by this Specific Plan of up to 25 percent, subject to approval of a ADVP. The number of required parking spaces may be reduced subject to the following:

1. Submittal of a parking demand study conducted by a licensed traffic engineer or other traffic professional acceptable to the City, and
2. Agreement to participate in the formation of a future parking assessment district or fee.

Parking Location

The location of parking spaces may be modified if the Planning Commission finds as follows:

1. The proposed location of the parking does not interfere with pedestrian connectivity and/or the pedestrian character of the Specific Plan.
2. Adequate landscaping buffers are provided to minimize the visual impact of the parking.
3. The parking does not eliminate opportunities for diagonal parking on Agoura Road or Cornell Road.
4. The location of the parking will support other policies and standards of the Specific Plan and will result in a better overall project.

Building Coverage and Building Size

The maximum building coverage of 60 percent and individual building size and/or tenant space of 30,000 square feet may be increased if the Planning Commission finds as follows:

1. The requested increase complies with the spirit and intent of the Specific Plan.
2. The building has been designed to incorporate both vertical and horizontal building articulation to help break up the building form, and is consistent with the design principles in the Specific Plan.

3. Adequate public open space is provided on the project site, given site constraints, building design and feasibility.

In no case may an individual building size and/or tenant space exceed 60,000 sf. Limitations on individual building size shall not apply to an individual building consisting of multiple tenant spaces as determined by the Director.

Zoning Ordinance Standards

For development standards not established as part of the Specific Plan, the standards in the Zoning Ordinance apply. The applicant may request a modification of the development standards of the Zoning Ordinance provided the Planning Commission determines that the development plan complies with the spirit and intent of the Specific Plan.

Any such modifications must be explicitly indicated in the development plan. Otherwise, the normal zoning regulations shall control. The Planning Commission may recommend such additional restrictions, conditions, or development schedules that are particularly applicable to the Agoura Village Development Permit.

Density Bonus

Residential dwellings are limited to a maximum number of dwelling units per Zone pursuant to Table 4.2 (Build-Out Summary). A density bonus increase of up to twenty five (25) percent of the maximum dwelling units allowed for each Zone may be requested by an applicant and granted pursuant to the following procedure and findings. As shown on Table 4-2, the density bonus shall be distributed as follows:

- 75 percent for additional residential units above retail (vertical)
- 25 percent for additional stand alone dwellings (horizontal)

The intent of this provision is to encourage more residential units over commercial. An example of how the density bonus would be calculated is

provided below. If a total of 95 units were allowed on the parcel, first determine the maximum number of allowed bonus units ($95 \times 25\% = 23$ units). The allowed density bonus of 23 units would then be distributed as follows: 17 units (23×0.75) would be allowed over commercial and 6 units (23×0.25) would be allowed as stand alone units.

Applicants may use either the State Affordable Housing Density Bonus provisions or the Agoura Village Density Bonus Program, not both.

The density bonus allowed for each Zone as shown in Table 4.2 of this plan may be granted in consideration of extraordinary contribution of public spaces and/or facilities or contribution to Transportation Impact Fees beyond required minimums by the Planning Commission subject to the following provisions:

1. Fee contribution: In addition to the minimum required City of Agoura Hills Traffic Impact Fee (TIF), applicant must pay an additional minimum 10 percent of the total (i.e., both residential and commercial) TIF to be eligible for the Residential Density Bonus program. Fees and corresponding residential bonus percentages shall be established by the Planning and Community Development Director and Public Works subject to approval by the City Council on recommendation of the Planning Commission up to the maximums stated herein. Payment of additional TIF Fees shall be considered mitigation to future traffic impacts for increases in density created by the bonus program. Revenues generated from residential density bonus fees shall be used in the Agoura Village Specific Plan area for transportation improvements directed by this Plan.
2. Public space and/or facilities contribution: In addition to the minimum requirements for public spaces and facilities specified by the Specific Plan, the City may approve or give consideration for the following contributions in lieu of a TIF fee contribution of at least 10%.

The applicant must provide cost detail to ensure that the cost of the proposed facilities is equal to or exceeds the required 10% increase in TIF contribution:

- Design and construction of any public plaza in excess of the minimum open space requirements of the specific plan including at least three of the following features:
 - Plaza or public space of at least 3,000 sf generally in the same location as depicted in the Specific Plan
 - Decorative paving, trees and landscaping and site furnishings in compliance with the Specific Plan
 - A water feature
 - Public art element
 - Public information kiosk
 - Other features and/or facilities as approved by the City Council on recommendation of the Planning Commission.
- Design and construction of any building or space within a building to be programmed and used as a community center, sheriff's substation, or any other civic use with the following features:
 - Space of at least 8,000 sf located in Zone A, B, D or E.
 - Kitchen area, large meeting room, flexible partitions, stage, storage facilities.
 - Other space programming and/or facilities as approved by the City Council on recommendation of the Planning Commission.

Allowable land Uses

A land use that is not listed in the Table 4.1 or is not shown on the corresponding district zoning plates for Zone A through F (and OS – Open Space District regulations for Zone G) is not allowed except as follows:

1. Required Findings: The Director may determine in writing that a proposed use is similar and compatible to a listed use and may be allowed upon making one or more of the following findings:
 - a. The characteristics of and activities associated with candidate uses are similar to one or more of the allowed uses and will not involve substantially greater intensity than the uses listed for that sub district;
 - b. The candidate uses will be consistent with the purpose and vision of the applicable sub district;
 - c. The candidate uses will be otherwise consistent with the intent of the specific plan;
 - d. The candidate uses will be compatible with the other uses listed for the applicable sub district.
2. The Director may refer the question of whether a proposed use is allowable directly to the Planning Commission or City Council for a determination at a public hearing.
3. A determination of the Director or Planning Commission may be appealed in compliance with the appeals procedure set forth in the City of Agoura Hills Municipal Code.

Nonconforming Uses

Existing uses that are not permitted or not listed in Table 4.1 in Chapter 4 (Land Use and Development Standards) are declared nonconforming uses. Please refer to the City of Agoura Hills Municipal Code for definitions and policies managing the following nonconforming uses:

- Continuances (continuing operation of nonconforming uses)
- Changing uses
- Terminations of nonconforming uses

Standards contained in Chapter 4 (Land Use and Development Standards) are mandatory requirements that must be satisfied for all new projects and building renovations for any increase in square footage over 200 square feet or any major modification of the exterior.

Exceptions to the nonconforming uses section shall be as follows:

- Properties that front onto Agoura Road in Zone A may add additional mixed use development including retail at the ground floor, office and/or residential space above along the Agoura Road frontage subject to the design guidelines and development standards in the Specific Plan, and review and approval of an Agoura Village Development Permit. Buildings shall have the mass and appearance of a two-story structure to screen any existing non-conforming uses. This section exempts compliance with Plan provisions regulating non-residential and mixed-use FAR maximums.

Environmental Impact Report

The final program Environmental Impact Report (FEIR) per the California Environmental Quality Act was certified by the City Council on June 14, 2006, along with adoption of the Mitigation Monitoring and Reporting Program (MMRP) and a Statement of Overriding Considerations. All mitigation measures outlined in the FEIR and the MMRP would apply to projects proposed in the Specific Plan area. Where the Specific Plan conflicts with the FEIR and MMRP, the FEIR and MMRP shall prevail. The MMRP is included in the Appendix.

Open Space Designations

The open space areas (Zone G) shown on Figure 4.8 that are located: south of Agoura Road and adjacent to the east side of Kanan Road in a rectangular area bounded by Zone A; along Medea Creek north of Agoura Road in the space between Zone C, Zone A north, and Zone D east; and the southern portion of the Specific Plan area. shall remain as open space per the requirements of City Ordinance No. 99-300, which requires voter approval to change these areas to non-open space uses.

Specific Plan Amendment

Applications for an amendment to this Specific Plan shall be reviewed in accordance with the provisions of Chapter 9820 of the Zoning Ordinance. Specific Plan amendments may deal with issues such as changes to permitted land uses, changes to circulation system, or any other issues relevant to the development of the property subject to this Specific Plan. In reviewing an amendment, the Planning Commission and City Council shall consider the following factors:

1. Whether the amendment is consistent with the General Plan.
2. Whether the intent and purpose of the Specific Plan is preserved.
3. Whether the amendment is sensitive to the maintenance and protection of the natural terrain and environment.
4. Whether traffic generation will exceed the acceptable levels of service established by the Specific Plan and City.
5. Whether circulation to and from the Specific Plan area will be maintained.
6. Whether public services and utilities will adequately serve the Specific Plan area.

Cost Recovery

Fees for the preparation of the Agoura Village Specific Plan, Environmental Impact Report and associated professional services will be recovered as specified in Section 9497.3 of Part 10 of Chapter 4 of the Agoura Hills Municipal Code.

Purpose and Overview

The purpose of the Cost Recovery Fee is to allow the City of Agoura Hills to recover the costs for preparing and implementing the Agoura Village Specific Plan, as well as other various related planning and environmental costs. The Agoura Village Specific Plan addresses the need to create a mix of uses that offer a town center and a pedestrian-friendly environment at a key location in the City, and results in enhanced public facilities and design features within the Agoura Village area. Since all property owners within the project area will benefit from these enhancements, the processing costs will be spread equitably among all property owners.

Applicability

The applicant for any building permit and/or discretionary land use permit for new construction on property located in whole or in part within the Agoura Village Specific Plan zone shall pay the Agoura Village Cost Recovery Fee at the same time and in the same manner as building permit and other development fees.

**APPENDIX I: MITIGATION
MONITORING AND
REPORTING PROGRAM**



AGOURA VILLAGE SPECIFIC PLAN
UPDATED REVISED AND RECIRCULATED
FINAL ENVIRONMENTAL IMPACT REPORT

MITIGATION MONITORING AND REPORTING PROGRAM

Approved by the City Council on October 22, 2008

Agoura Village Specific Plan Updated Final Revised and Recirculated EIR
Mitigation Monitoring and Reporting Program

Mitigation Measure/Condition of Approval	Action Required	When Monitoring to Occur	Monitoring Frequency	Responsible Agency or Party	Compliance Verification		
					Initial	Date	Comments
AESTHETICS							
<p>AES-1 Retaining Wall Design. In the event any proposed retaining walls are visible from designated scenic roadways, the City's Architectural Review Board shall determine whether they are consistent with the City's Architectural Design Standard and Guidelines (1992). If any wall is found to be inconsistent with the Guidelines, the Architectural Review Board shall recommend additional design features to bring the wall(s) into compliance. Possible design features may include the use of textured retaining walls with more natural features, such as those that simulate rocks or boulders. Additionally, design features may include the planting of landscape vegetation along the wall facing south toward the freeway. This landscape vegetation should include plants that provide vertical wall coverage, in order to enhance the visual character of the wall and break up the area of the wall that is visible from scenic corridors. Such retaining wall, landscaping and other related design features shall be shown on the project plans and verified by City Planning and Community Development Department Staff prior to issuance of a Grading or Building Permit.</p>	<p>PCD to require ARB review of projects with retaining walls visible from scenic roadways; ensure that design features are included consist with Architectural Design Standard and Guidelines as appropriate.</p>	<p>-Prior to approval of future projects -At site inspection.</p>	<p>-Once per project application. -At least once, as required.</p>	<p>PCD</p>			

Key: PCD – City of Agoura Hills Planning and Community Development Department
 B&S – City of Agoura Hills Building and Safety

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<p>AES-3 Avoidance of Knolls. The applicant shall avoid development, removal, or reduction (to include grading or blasting) of that knoll located south and east of the intersection of Agoura and Kanan Road. Although development of the site is unlikely, given it is zoned as Open Space and would require a vote of the people in order to be rezoned, the applicant shall avoid this area in order to avoid substantially modifying a scenic resource. Additionally, the applicant shall minimize grading (subject to approval of City Community Planning and Development Department) of the knoll located south and east of the intersection of Agoura and Cornell Road. Although development and minor modifications would be allowed on the knoll, the majority of the knoll should be preserved.</p>	<p>Ensure that any development or earthwork avoids or minimizes disturbance of the respective knolls as specified.</p>	<p>-Prior to approval of future projects -At site inspection.</p>	<p>-Once per project application. -At least once, as required.</p>	PCD			
<p>AES-4 Glare Reduction. Project design and architectural treatments shall incorporate additional techniques to reduce glare, such as:</p> <ul style="list-style-type: none"> • Use of low reflectivity glass; • Use of plant material along the perimeter of structures to soften views; and, • Brush-polishing metal surfaces and/or use of metal surfaces that are not highly reflective. <p>Plans for new development shall indicate the architectural treatments and/or landscaping to be used in order to reduce glare that could be generated by new development. Plans shall be reviewed by City staff, the Architectural Review Panel, and the City's Architectural consultant for compliance with this standard.</p>	<p>Ensure that future projects incorporate glare reduction techniques as described; that such techniques are shown on plans and reviewed by the ARB and the City's Architectural consultant for compliance.</p>	<p>-Prior to approval of future projects -At site inspection.</p>	<p>-Once per project application. -At least once, as required.</p>	PCD			
<p>AES-5 Each project applicant would be required to obtain a permit from the City and to comply with the provisions of the permit, prior to the approvals of removal</p>	<p>Require permits for oak tree removal.</p>	<p>When oak tree removal is proposed.</p>	<p>Once per project application.</p>	PCD			

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					Initial	Date	Comments
of oak trees.							
AIR QUALITY							
AQ-1(a) Fugitive Dust Control Measures: <ul style="list-style-type: none"> Water trucks shall be used during construction to keep all areas of vehicle movements damp enough to prevent dust from leaving the site. At a minimum, this will require twice daily applications (once in late morning and once at the end of the workday). Increased watering is required whenever wind speed exceeds 15 mph. Grading shall be suspended if wind gusts exceed 25 mph. The amount of disturbed area shall be minimized and onsite vehicle speeds shall be limited to 15 mph or less. If importation, exportation and stockpiling of fill material is involved, earth with 5% or greater silt content that is stockpiled for more than two days shall be covered, kept moist, or treated with earth binders to prevent dust generation. Trucks transporting material shall be tarped from the point of origin or shall maintain at least two feet of freeboard. After clearing, grading, earth-moving or excavation is completed, the disturbed area shall be treated by watering, revegetation, or by spreading earth binders until the area is paved or otherwise developed. All material transported off-site shall be securely covered to prevent excessive amounts of dust. 	Require fugitive dust control measures for future development projects, as specified.	-Prior to approval of future projects -At site inspection.	-Once per project application. -At least once, as required.	PCD			
AQ-1(b) NO_x Control Measures: <ul style="list-style-type: none"> When feasible, electricity from temporary power poles on site shall be utilized rather than temporary diesel or gasoline generators; When feasible, on site mobile equipment shall 	Require NO _x control measures for future development projects, as specified.	-Prior to approval of future projects -At site	-Once per project application. -At least once,	PCD			

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be fueled by methanol or natural gas (to replace diesel-fueled equipment), or, propane or butane (to replace gasoline-fueled equipment) <ul style="list-style-type: none"> Aqueous Diesel Fuel or biodiesel (B20 with retarded fuel injection timing), if available, shall be used in diesel fueled vehicles when methanol or natural gas alternatives are not available. 		inspection.	as required.				
AQ-1(c) VOC Control Measure: <ul style="list-style-type: none"> Low VOC architectural and asphalt coatings shall be used on site and shall comply with AQMD Rule 1113-Architectural Coatings. 	Require that low VOC coatings are used for future development projects, as specified.	-Prior to approval of future projects -At site inspection.	-Once per project application. -At least once, as required.	PCD			
AQ-1(d) Ozone Precursor Control Measures: <ul style="list-style-type: none"> Equipment engines should be maintained in good condition and in proper tune as per manufacturer's specifications; Schedule construction periods to occur over a longer time period (ie lengthen from 60 days to 90 days) during the smog season so as to minimize the number of vehicles and equipment operating simultaneously; and Use new technologies to control ozone precursor emissions as they become readily available. 	Require Ozone Precursor Control Measures for future development projects, as specified.	-Prior to approval of future projects -At site inspection.	-Once per project application. -At least once, as required.	PCD			
AQ-2 Decrease Emissions of diesel particulate matter during site grading by implementing one of the following four measures. <ul style="list-style-type: none"> Construction contractors shall not operate more than two pieces of heavy-duty diesel-powered equipment within 600 feet of any residence at any time. Construction contractors shall use biodiesel fuel in all on-site diesel-powered equipment. Biodiesel that is blended with low sulfur diesel fuel shall be used if 	Ensure that one of the specified measures is implemented during grading for future projects.	-Prior to approval of future projects -At site inspection.	-Once per project application. -At least once, as required.	PCD			

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<p>available.</p> <ul style="list-style-type: none"> • Construction contractors shall use only Tier 2 diesel-powered earth moving equipment. • At least 80% of the diesel-fueled construction equipment in terms of brake-horsepower shall have DPFs installed, or all equipment shall be equipped with diesel oxidation catalysts. • Construction contractors shall limit the movement of large trucks to off-peak commute hours. 							
<p>AQ-3(a) Energy Consumption. Onsite structures shall reduce energy consumption by at least 20% below current Federal guidelines as specified in Title 24 of the Code of Federal Regulations. Potential energy consumption reduction measures include, but are not limited to, the use of photovoltaic roof tiles, installation of energy efficient windows, and the use of R-45 insulation in the roof/attic space of all onsite structures.</p>	<p>Ensure that future structures include measures to reduce energy consumption by at least 20% below current Federal guidelines.</p>	<p>-Prior to approval of future projects -At site inspection.</p>	<p>-Once per project application. -At least once, as required.</p>	PCD			
<p>AQ-3(b) Landscape Equipment. Multi-family residential developments shall be encouraged to utilize electrical powered landscape maintenance equipment, and exterior outlets shall be installed at the front and rear of residences.</p>	<p>Encourage use of electrical powered landscape maintenance equipment for future multi-unit residential projects, and require provision of exterior outlets to facilitate their use.</p>	<p>Prior to approval of future projects.</p>	<p>Once per project application.</p>	PCD			
<p>AQ-3(c) Shade Trees Shade trees shall be planted to shade onsite structures to the greatest extent possible in summer, reducing indoor temperatures, and reducing energy demand for air conditioning. The City's ARB shall review project landscaping plans for consistency with this mitigation measure.</p>	<p>Require shade trees in future projects to shade structures, and that the ARB review landscaping plans for consistency.</p>	<p>-Prior to approval of future projects -At site inspection.</p>	<p>-Once per project application. -At least once, as required.</p>	PCD			
<p>AQ-3(d) Bus Stops. Applicants shall provide bus stops</p>	<p>Require that bus stops</p>	<p>-Prior to</p>	<p>-Once per</p>	PCD			

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within the Specific Plan Area. The number to be constructed will be determined in consultation with the City Traffic Engineer and the local transit agencies. Bus stops shall meet the requirements of the transit agency providing service to the City and shall include street furniture that provides shelter for passengers.	meeting City and transit agency standards and including passenger shelters as specified be provided in future projects in the Specific Plan Area as appropriate.	approval of future projects -At site inspection.	project application. -At least once, as required.				
AQ-4 Equestrian Center and Trail Maintenance Plan. As part of the City's feasibility study for an equestrian center within the Specific Plan area, the City shall include provisions for a maintenance plan of both the equestrian center and related trails. The maintenance plan shall include the following measures, at a minimum: <ul style="list-style-type: none"> Organic debris/waste shall be properly disposed of or sold offsite on a regular basis, BMP's shall be instituted to prevent dust from moving offsite, BMP's (to include necessary bioswales or erosion control measures) shall be instituted to prevent organic waste, or associated nutrients from organic waste, from entering nearby water bodies. 	Ensure that the City's feasibility study for an equestrian center within the Specific Plan area includes provisions for center and trail maintenance plans as specified.	Prior to release of the feasibility study.	Once per study draft.	PCD			
BIOLOGY							
BIO-1(a) Sensitive Plant Survey and Protection Plan. Prior to approval of individual development applications within the residual natural areas of Zones A south, B, E, and F, surveys for sensitive plant species, including but not limited to Agoura Hills dudleya and Lyon's pentachaeta, should be performed by a qualified plant ecologist. These surveys shall be performed during the blooming period (April - June). If a sensitive species is found, avoidance shall be required unless the applicant provides substantial documentation that avoidance would	Require sensitive plant surveys be performed as specified in the measure for proposed development within the areas listed, and mitigation and monitoring as specified where appropriate, including avoidance of Lyon's pentachaeta and Agoura	-Prior to approval of future projects -During construction and at site inspection.	-Once per project application. -At least once, as required.	PCD			

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<p>not be feasible or would compromise the objectives of the Specific Plan. For Lyon's pentachaeta and Agoura Hills dudleya, avoidance is defined as a minimum 200 foot setback unless an active maintenance plan is implemented for the known occurrence. With implementation of an active maintenance and management program, the buffer width may be reduced further based on review and approval by the jurisdictional agencies (USFWS and/or CDFG). For other sensitive species avoidance shall be determined based on the specific plant pursuant with the recommendations of a qualified plant ecologist, and with the coordination of USFWS and/or CDFG for state or federally listed plants. The maintenance and management plan must be approved by the appropriate jurisdictional agencies prior to issuance of a grading permit.</p> <p>If avoidance is not feasible, on-site mitigation is preferred if suitable, unoccupied, habitat is present that can be isolated from human disturbance. Otherwise, an offsite location would be considered; the Ladyface Mountain Specific Plan area may contain appropriate habitat and may be a preferred location. A mitigation restoration plan shall be prepared by a qualified plant ecologist that identifies the number of plants to be replanted and the methods that will be used to preserve this species in the on- or off-site mitigation location. The plan shall also include a monitoring program so that the success of the effort can be measured. Restoration efforts shall be coordinated with applicable federal, state, and local agencies. The required level of success for Agoura Hills dudleya and Lyon's pentachaeta shall be defined at a minimum as a demonstration of five consecutive years, or a period as deemed appropriate by the permitting agencies (USFWS and/or CDFG), of growth of a population equal to or greater than that which would be</p>	<p>Hills dudleya, unless a successful mitigation replacement population is established in accordance with the appropriate success period (as determined by the permitting agencies). Ensure that restoration efforts are coordinated with applicable federal, state, and local agencies.</p>						

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lost due to the project. This level of success shall be achieved prior to removal of the impacted population. Success criteria for other sensitive species will be determined on an individual basis pursuant with the recommendations of a qualified plant ecologist, and with the coordination of USFWS and/or CDFG for state or federally listed plants. When applicable the mitigation restoration plan shall be submitted to the appropriate regulatory agencies for review and approval, with the approved plan then submitted to the City of Agoura Hills prior to issuance of a grading permit for the area of concern.							
BIO-1(b) Sensitive Wildlife Survey. Not more than two weeks prior to ground disturbing construction within the Specific Plan area, a preconstruction survey for the two-striped garter snake, burrowing owl, western pond turtle, sensitive bat species, and any other special-status species shall be conducted by a qualified biologist and submitted to the City Planning and Development Department prior to beginning construction and/or commencement of any disturbance. If a species is found, avoidance is the preferred mitigation option. If avoidance is not feasible these species shall be captured, when possible, and transferred to adjacent appropriate habitat within designated open space areas either onsite or directly adjacent to the project area. This shall be performed only by a CDFG approved biologist. The CDFG and City of Agoura Hills shall be formally notified and consulted regarding the presence of these species onsite. If a federally listed species is found prior to grading of the site, the USFWS shall also be notified. Only a USFWS approved biologist would be allowed to capture and relocate these animals.	Require sensitive wildlife surveys as specified in the measure for proposed development within the Specific Plan area, and mitigation and monitoring as specified where appropriate. Ensure that a CDFG-approved biologist perform surveys, and that if a federally listed species is found, the USFWS is notified and a USFWS-approved biologist carry out any capture and relocation of such animals.	-Prior to approval of future projects -During construction and at site inspection.	-Once per project application. -At least once, as required.	PCD			

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<p>BIO-1(c) Bird Nesting Surveys. If vegetation clearing (including tree pruning and removal) or other project construction is to be initiated during the bird breeding season (February 1 through August 31), pre-construction/grading surveys shall be conducted by a qualified ornithologist (a person with a biology degree and/or established skills in bird recognition). Surveys shall begin 30 days prior to initial disturbance activities and shall continue weekly, with the last survey being conducted no more than three days prior to the initiation of clearance/construction work. If bird species are observed nesting within 500 feet of construction/grading areas, all construction or grading activities will be postponed or halted at the discretion of the biologist until the nest is vacated and the juveniles have fledged.</p> <p>Limits of construction to avoid a nest should be established in the field with flagging and stakes or construction fencing. This distance shall be at least 300 feet for raptors and at least 100 feet for all other bird species. Construction personnel should be instructed on the sensitivity of the area. The applicant should record the results of the recommended protective measures described above to document compliance with applicable State and federal laws pertaining to the protection of native birds.</p>	Require bird nesting surveys as specified in the measure for proposed development within the Specific Plan area, and mitigation and monitoring as specified where appropriate.	<p>-Prior to approval of future projects</p> <p>-During construction and at site inspection.</p>	<p>-Once per project application.</p> <p>-At least once, as required.</p>	PCD			
<p>BIO-2(a) Buffer Zones. Except in cases of Lyon's pentachaeta and/or Agoura Hills Dudleya, which are addressed in MM BIO-1(a), a minimum buffer zone of 50-100 feet of native vegetation shall be maintained between urban development and adjacent sensitive native habitats. This includes those areas located along the unchannelized portions of Medea and Lindero Canyon Creeks within the Specific Plan boundaries.</p>	Require incorporation of a appropriate habitat buffer areas for native vegetation for future projects.	<p>-Prior to approval of future projects</p> <p>-At site inspection.</p>	<p>-Once per project application.</p> <p>-At least once, as required.</p>	PCD			

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Such vegetation should be sensitive to, and similar in nature to, the natural environment surrounding the sensitive native habitats. A minimum buffer of 50 feet (or greater if required by the CDFG) from the top of bank and/or edge of riparian cover (whichever is greater) shall be established for the protection of southwestern pond turtle where preferred nesting habitat (exposed, southerly-facing slopes vegetated with open scrub or sparse grassland vegetation, dense soils with a high silt and clay fraction, and less than 25% slope) is present. No heavy equipment or ground disturbance shall enter the buffer zone during the nesting period of SWPT (April-August). Further, equestrian trails shall be located no less than 10 to 20 (preferred) feet from the edge of the exterior riparian canopy.							
BIO-2(b) Native Grassland Protection. Prior to approval of individual development applications within the southern portion of the Specific Plan area, surveys for native grasslands shall be performed by a qualified biologist (with acceptance by the City Planning and Community Development Department Staff). If native grasslands are found, avoidance shall be required unless the applicant provides substantial documentation that avoidance would not be feasible or would compromise the objectives of the Specific Plan. Avoidance shall be planned and enforced with a Native Grassland Protection Program . If the applicant demonstrates that avoidance would not be feasible or would compromise the objectives of the Specific Plan, on-site mitigation would be required if suitable habitat is present and can be isolated from human disturbance. In this event, a Native Grassland Restoration Plan shall be prepared and implemented.	Require native grassland surveys for future development proposals and native grassland protection programs, including avoidance and mitigation as appropriate, where warranted. Protocols for surveys and protection/restoration are included in the mitigation measure.	-Prior to approval of future projects -During construction and at site inspection.	-Once per project application. -At least once, as required.	PCD			

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<p>Native Grassland Protection Program. If native grasslands are found onsite and avoidance is feasible, a native grassland protection program shall be prepared by a qualified biologist. The protection program shall be submitted for review and approval as part of the application process with the City Planning and Development Department. In addition, final plans shall be subject to review and approval by the City Planning and Community Development Department prior to issuance of a grading permit. The protection program shall include, but not be limited to, the following components:</p> <ul style="list-style-type: none"> • A qualified biologist shall map the current extent of habitat; and • The location of native grassland habitat outside of the construction footprint shall be fenced in the field. Fencing shall be depicted on final grading and building plans. The location of the habitat and fencing shall be done under the direction of a qualified biologist (with acceptance by the City Planning and Community Development Department Staff); and • All ground disturbances, including grading for buildings, accessways, easements, subsurface grading, and utilities shall be prohibited within the fenced native grassland area. <p>Native Grassland Restoration Plan. If avoidance is not feasible, on-site mitigation is preferred if suitable habitat is present that can be isolated from human disturbance. In this event, a restoration plan shall be prepared by a qualified plant ecologist that identifies the location and acreage to be replanted and the methods that will be used to preserve this community in that location. The plan shall also include a monitoring program so that the</p>							

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<p>success of the effort can be measured. The required level of success, at a minimum, shall be defined as a demonstration of three consecutive years of at least 50% native grass dominance within the mitigation area. If off-site mitigation is proposed, the Ladyface Mountain Specific Plan area may contain appropriate habitat and may be a preferred location. Restoration efforts shall be coordinated with applicable federal, state, and local agencies (including LA County Fire Department). The restoration plan shall be submitted for review as part of the application process with the City Planning and Development Department. In addition, final plans shall be subject to review and approval by the City Planning and Development Department prior to issuance of a Grading Permit.</p> <p>Native grassland habitat shall be replaced at a minimum ratio of three to one for native grassland lost and shall utilize native species from onsite habitats. Target sites for mitigation plots shall be sampled for soil type and habitat criteria sufficient for the establishment and growth of the native grassland lost. No species identified as invasive on the CNPS, Channel Islands Chapter Invasive Plants List (1997) shall be utilized in the landscape plans. The plan shall include, but not be limited to, the following components:</p> <ul style="list-style-type: none"> • Performance criteria (i.e., what is an acceptable success level of revegetation to mitigate past impacts); • Monitoring effort (i.e., who is to check on the success of the revegetation plan, and how frequently); • Contingency planning (i.e., if the effort fails to reach the performance criteria, what remediation steps need to be taken); • Irrigation method/schedule (i.e., how much water is 							

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<p>needed, where, and for how long);</p> <ul style="list-style-type: none"> Plant species, seed mixes, weed suppression and planting methodology <p>From preliminary observations, it appears that potential target areas to perform mitigation for the loss of native grassland exist on the northern slopes of Ladyface Mountain, within the open space of Zone G (the area formerly identified in the 1996 Creekside EIR as valley needlegrass grassland and located south of Lindero Canyon Creek) in the southwest corner of the Specific Plan boundary. These areas need testing to confirm that they meet the soil and habitat requirements for native grassland species. If sufficient mitigation area does not exist onsite, off site mitigation or in lieu fees to an off site local or regional mitigation bank acceptable to the City of Agoura Hills shall be done.</p>							
<p>BIO 2(c) Southern Willow Scrub/ Southern Arroyo Willow Riparian Protection. Based on a review of pending development applications near Lindero Canyon Creek, it is anticipated that the existing southern willow scrub/ southern arroyo willow riparian may be encroached upon; however, avoidance of these areas is required. If avoidance is feasible, the following Riparian Habitat and Creek Protection Program shall be implemented in order to reduce impacts to this sensitive community. If the applicant demonstrates that avoidance would not be feasible or would compromise the objectives of the Specific Plan, on-site mitigation is preferred and shall be implemented through a Riparian Habitat Restoration Plan, as outlined below.</p> <p>Riparian Habitat and Creek Protection Program. A riparian habitat and creek protection program shall be</p>	<p>Require southern willow scrub/ Southern Arroyo Willow Riparian protection, including avoidance and mitigation as appropriate, where warranted. Protocols for protection/restoration are included in the mitigation measure.</p>	<p>-Prior to approval of future projects</p> <p>-During construction and at site inspection.</p>	<p>-Once per project application.</p> <p>-At least once, as required.</p>	PCD			

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<p>prepared and implemented for any future developments proposed within the Specific Plan area adjacent to Lindero Canyon or Medea Creeks. These shall be prepared by a qualified biologist (with acceptance by the City Planning and Community Development Department Staff) and shall include specific measures as dictated by CDFG. The program shall, to the extent feasible, avoid encroachment into any riparian areas. The program shall include, but not be limited to, the following components:</p> <ul style="list-style-type: none"> • Riparian areas shall be indicated and fenced off on all grading and construction plans. The location of the habitat and fencing off shall be done under the direction of a qualified biologist (with acceptance by the City Planning and Community Development Department Staff). Construction personnel shall be informed of the sensitivity and location of riparian habitat on the project site; and • All ground disturbances including grading for buildings, accessways, easements, subsurface grading, and utilities shall be prohibited within the fenced riparian area. <p>The protection program shall be submitted for review as part of the application process with the City Planning and Community Development Department. In addition, the final plans shall be subject to review and approval by the City Planning and Community Development Department prior to the issuance of a Grading Permit.</p> <p>Riparian Habitat Restoration Plan. However, if avoidance is not feasible, on-site mitigation is preferred over off-site mitigation but both mitigation measures could be effective at reducing the impacts to less than significant. If avoidance is not feasible, a restoration plan</p>							

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<p>shall be prepared by a qualified plant ecologist. The preferred area to perform mitigation for the loss of riparian forest exists within the southern reach of the channelized and concrete lined portion of Medea Creek, located directly south of Agoura Road and also in the vicinity of Lindero Canyon Creek. If development were to encroach upon this sensitive community, the appropriate permits would be necessary from the Army Corps of Engineers, the California Department of Fish and Game, and the Los Angeles Regional Water Quality Control Board. Individual applicants for projects located south of Agoura Road and that contain riparian habitat areas, shall submit a Riparian Habitat Restoration Plan for review by the City Planning and Community Development Department and, as necessary, a City approved biologist or qualified landscape specialist, as part of the initial project application. Riparian habitat shall be replaced at a minimum ratio of 2.0 acres for every 1.0 acre of riparian habitat lost. However, additional mitigation may be required by the CDFG. The restoration plans shall include, but not be limited to, the following components:</p> <ul style="list-style-type: none"> • Performance criteria (i.e., what is an acceptable success level of revegetation to mitigate past impacts); • Monitoring effort (i.e., who is to check on the success of the revegetation plan, and how frequently); • Contingency planning (i.e., if the effort fails to reach the performance criteria, what remediation steps need to be taken); and • Irrigation method/schedule (i.e., how much water is needed, where, and for how long). <p>The required level of success, at a minimum, shall be defined as a demonstration of three consecutive years of</p>							

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growth of a population double the size of that which would be lost due to the project. The final restoration plan shall be subject to review and approval by the City Planning and Community Development Department prior to Grading Permit issuance.							
<p>BIO-3(a) Oak Tree Protection and Preservation. Individual project applicants shall submit the results of an oak tree survey and an Oak Tree Report, including an Oak Tree Preservation Program, for review and approval by the City's oak tree consultant as part of the project application. Individual projects shall be developed and operated in compliance with the approved Oak Tree Preservation Program and any other conditions determined to be necessary by the City oak tree consultant. The program shall include, but not be limited to, the following components:</p> <ul style="list-style-type: none"> • No grading or development shall occur within 5 feet from the driplines of oak trees that occur in the construction area. • All specimen oak trees within 25 feet of proposed ground disturbances shall be temporarily fenced with chain-link or other material satisfactory to the City throughout all grading and construction activities. The fencing shall be installed six feet outside the dripline of each specimen oak tree, and shall be staked every six feet. • No construction equipment shall be parked, stored or operated within six feet of any specimen oak tree dripline. • No fill soil, rocks, or construction materials shall be stored or placed within six feet of the dripline of a specimen oak tree (pervious paving and other materials are allowed, as approved by the City). 	Require oak tree surveys, reports and preservation programs for future development projects. Ensure review of these documents by the department's oak tree consultant.	<p>-Prior to approval of future projects</p> <p>-At site inspection.</p>	<p>-Once per project application.</p> <p>-At least once, as required.</p>	PCD			

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<ul style="list-style-type: none"> No artificial surface, pervious or impervious, shall be placed within six feet of the dripline of any specimen oak tree, except for project access roads. Any roots encountered that are one inch in diameter or greater shall be cleanly cut. This shall be done under the direction of a City approved arborist/oak tree consultant. Any trenching required within the dripline or sensitive root zone of any specimen tree shall be done by hand. In addition, trenching in the protected zone needs to preserve roots over 1 inch by tunneling. No permanent irrigation shall occur within the dripline of any existing oak tree. Any construction activity required within three feet of a specimen oak tree's dripline shall be done with hand tools. Any construction activity required within three feet of a specimen oak tree's dripline shall be done with hand tools. 							
<p>BIO-3(b) Grading Plan. The number of oak trees requiring removal and the number of trees that will be encroached upon by grading and project development shall be confirmed by the City's oak tree consultant with the final grading plan. The plan shall also indicate requirements for retaining walls, tree wells, tree drainage requirements, and pruning as part of the plan.</p>	Require that oak tree information be shown on final grading plans for future projects.	Prior to approval of future projects.	Once per project application.	PCD			
<p>BIO-3(c) Oak Tree Replacement. For impacts involving 10 percent or less of oak tree removal resulting from grading and project development, each oak tree shall be replaced with specimen oak trees of the same species as the tree that was removed at a ratio and dimension specified in the City's Zoning Ordinance. This mitigation is to occur onsite. For impacts involving greater than 10</p>	Ensure that the specified oak replacement criteria and ratios are applied to future projects involving oak tree removal.	<p>-Prior to approval of future projects</p> <p>-At site inspection.</p>	<p>-Once per project application.</p> <p>-At least once, as required.</p>	PCD			

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percent of oak tree removal resulting from grading and project development, mitigation shall either be onsite with the requirements as listed above, or an in-lieu fee may be paid to the City to be used to acquire land and/or install oak trees on another site, preferably in as close proximity to the area of removal as possible. The sum of the calipers of all oak trees planted must be at least equal to that removed. The locations of the replanted trees shall be indicated on the project plans submitted to the City for review by the City's oak tree consultant. Trees shall be planted so that mature trees will have a continuous canopy. Every attempt shall be made to plant oak trees according to species-specific habitat requirements: valley oaks at lower elevations in alluvial soils; and coast live oaks on mesic north facing slope locations. Each oak tree removed by grading and project development shall be replaced with two 36 inch box and two 24 inch box specimen oak trees of the same species as the tree that was removed. Additionally, all naturally occurring native vegetation in the areas proposed for oak tree mitigation shall be identified. This includes surveys for ephemeral plants and bulbs. Oak tree planting shall not cause the removal or destruction of existing native vegetation without replacement in the same locations.							
BIO-3(d) Oak Planting Arrangements. Where appropriate pursuant to the recommendations of the City's oak tree consultant, replacement oaks for the removal of individual oak trees shall be clustered in an attempt to replace oak woodland habitat removed. Trees shall be planted so that mature trees will have a continuous canopy. Every attempt shall be made to plant oak trees according to species-specific habitat requirements: valley oaks at lower elevations in alluvial soils and coast live oaks on mesic north facing slope	Ensure that the specified oak replacement standards are applied to future projects involving oak tree removal.	-Prior to approval of future projects -At site inspection.	-Once per project application. -At least once, as required.	PCD			

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locations.							
<p>BIO-4(a) Replacement Ratio. Federal and State protected wetland habitat shall be replaced at a minimum ratio of 2.0 acres of habitat, at the same or greater quality, for every 1.0 wetland acre removed. Replacement shall be at an Agoura Hills Planning and Community Development Department approved location or by providing adequate funding for the replacement of wetland habitat to an organization currently conducting restoration of wetland habitat. The organization and its activities are to be approved by an Agoura Hills Planning and Community Development Department approved biologist. Two areas located within the Specific Plan boundaries shall be considered for mitigation credit. That portion of Lindero Canyon Creek located between Agoura Road and Kanan Road is the preferred mitigation location for impacts to other wetland areas within the project area. This restoration effort would include restoring the channel to a more natural state. Improvement of the unchannelized portion of Medea Creek, located south of Agoura Road, shall be considered as an alternate location for mitigation and wetland restoration.</p>	<p>Ensure that the specified wetland replacement ratios are applied to future projects where appropriate, and that the identified mitigation credit and restoration areas are used when warranted.</p>	<p>-Prior to approval of future projects -At site inspection.</p>	<p>-Once per project application. -At least once, as required.</p>	PCD			
<p>BIO-4(b) Wetland Restoration Plan. For projects that may adversely impact wetland areas, individual project applicants shall submit a wetland creation or restoration plan for review and approval by an Agoura Hills Planning and Community Development Department staff and, as necessary, a City approved biologist or qualified landscape specialist, as part of the initial application. The final restoration plan shall be submitted for City review and approval prior to Grading Permit issuance. The plan shall include, but not be limited to the following</p>	<p>Require wetland creation or restoration plans as specified in the measure where projects would result in wetland impacts.</p>	<p>-Prior to approval of future projects -At site inspection.</p>	<p>-Once per project application. -At least once, as required.</p>	PCD			

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components: <ul style="list-style-type: none"> • Performance criteria (i.e., what is an acceptable success level of revegetation to mitigate past impacts); • Monitoring effort (i.e., who is to check on the success of the revegetation plan, and how frequently); • Contingency planning (i.e., if the effort fails to reach the performance criteria, what remediation steps need to be taken); and • Irrigation method/schedule (i.e., how much water is needed, where and for how long). 							
BIO-4(c) City Approval. For projects where wetland areas are affected, the individual project applicants shall demonstrate to the City of Agoura Hills that the requirements of agencies with jurisdiction over wetlands onsite can be met prior to obtaining grading permits. This will include, but not be limited to, consultation with those agencies, securing the appropriate permits, waivers or agreements, and arrangements with a local or regional mitigation bank including in lieu fees, as needed.	Require applicants for projects that would affect wetlands to demonstrate to the City compliance with regulations of other agencies having jurisdiction over wetlands.	Prior to approval of grading permits for future projects.	Once per project application.	PCD			
BIO-4(d) Riparian Habitat Preservation and Restoration. Refer to BIO-2(c) above.	Refer to BIO-2(c) above.						
BIO-4(e) Fencing. Solid barrier fencing onsite shall be prohibited around areas that border open spaces or routes of animal movement, specifically riparian areas. Fencing in these areas shall consist of "ranch style" post fencing. Fencing shall allow at least one-foot of clearance above ground to permit wildlife movement. Fencing between creekside trails and the creeks shall be designed to limit human entry into significant habitat. Such fencing or vegetative barrier shall be at least four feet in height and shall be planted with spinescent plants	Require fencing proposed around areas that border open spaces or routes of animal movement to allow for wildlife movement as specified.	-Prior to approval of future projects -At site inspection.	-Once per project application. -At least once, as required.	PCD			

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such as wild rose, blackberry, or other suitable native species in a dense bramble.							
<p>BIO-4(f) Corridor Lighting. The following low-light design features shall be implemented throughout the Specific Plan area, and shown on the individual project plans submitted as part of the application.</p> <ul style="list-style-type: none"> • Streetlight poles shall be of an appropriate height to reduce the glare and pooling of light into open space and corridor areas, and • Street light elements shall be recessed or hoods shall be used to reduce glare impacts on open space and corridor areas, and • All exterior lighting shall be low sodium lights, low intensity, shielded, and directed away from the drainage/wildlife corridors corridor. 	Require the specified low-light design features for projects in the plan area, and that these be shown on project plans.	<p>-Prior to approval of future projects</p> <p>-At site inspection.</p>	<p>-Once per project application.</p> <p>-At least once, as required.</p>	PCD			
<p>BIO 6(a) Coastal Sage Scrub Habitat Survey. As part of the sensitive plant surveys required under Mitigation Measure BIO-1(a), prior to approval of individual development applications within the residual natural areas of Zones A south, B, E, and F, surveys for sensitive plant species shall also include surveys and consideration of adjacent areas of Coastal Sage Scrub habitat. A qualified biologist shall determine the condition of such habitat and whether it would be considered of "high value." Any areas identified as "high value" Coastal Sage Scrub habitat shall mitigate for disturbed (including disturbance for fuel modification) or removed CSS habitat at a minimum 1:1 ratio. Coastal Sage Scrub habitat with known occurrences of sensitive (endangered or threatened) species shall be mitigated at a minimum 2:1 ratio.</p>	Require sensitive plant surveys in the areas identified to include surveys and consideration of adjacent areas of Coastal Sage Scrub habitat, and projects to include mitigation and monitoring as specified where appropriate.	<p>-Prior to approval of future projects</p> <p>-At site inspection.</p>	<p>-Once per project application.</p> <p>-At least once, as required.</p>	PCD			

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BIO 6(b) Fuel Modification Areas. Landscaping within fire clearance zones shall include native species indigenous to the area of disturbance. Modification of fire hazard fuels shall be limited to hand thinning of individual shrubs, clearing dead fuel, replanting with fire-resistant plants indigenous to the area, or other methods to attain fire safety while producing a viable natural and native vegetation community. No species identified as invasive on the CNPS, Channel Islands Chapter Invasive Plants List (1997) shall be utilized in the landscape plans and all landscaping plans shall be approved by the City and the County Fire Department.	Require that the specified standards be applied to landscaping within identified fire clearance zones. Ensure landscape plan review and approval by PCD and the County Fire Department.	-Prior to approval of future projects -At site inspection.	-Once per project application. -At least once, as required.	PCD			
GEOLOGY							
GEO-1(a) Building Design. All buildings shall be engineered to withstand the expected design basis ground acceleration that may occur at the project site. All critical facilities shall be designed to withstand the upper bound earthquake ground motion. The design shall take into consideration the most current and applicable seismic attenuation methods that are available. All onsite structures shall comply with applicable provisions of the California Building Code and Chapter 1 of Article 8 of the Agoura Hills Municipal Code. Compliance with these requirements shall be verified by the City Building Official prior to issuance of a Building Permit or Grading Permit.	Require that structures are built to the standards listed and comply with the CBC and Municipal Code.	-Prior to approval of building or grading permits for future projects. -At site inspection.	-Once per project application. -At least once, as required.	PCD B&S			
GEO-1(b) Geotechnical Recommendations. Future development shall require, and comply with, all recommendations contained in site-specific geologic, geotechnical, and structural design studies prepared for subsequent development activities. Subsequent subsurface investigations shall determine the possible presence of seismically induced hazards and appropriate	Ensure that, where required, geologic, geotechnical, and structural design studies determine the presence of seismically induced hazards, as well as other factors, and	-Prior to approval of building or grading permits for future projects.	-Once per project application.	PCD B&S			

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means of mitigating such hazards. Recommendations contained in these site-specific studies shall be reviewed and approved by the City Building Official and incorporated in to final grading and structural design plans, as deemed appropriate by the City Building Official. At a minimum, any buildings considered essential facilities, as defined in the Uniform or California building codes, shall be designed to withstand upper bound earthquake ground motion. All on-site structures shall comply with applicable provisions of the 1997 Uniform Building Code and the 1998 California Building Code. The calculated design base ground motion for the site shall take into consideration the soil type, potential for liquefaction, and the most current and applicable seismic attenuation methods that are available.	appropriate means of mitigating hazards. Ensure that development adheres to recommendations of such studies as deemed appropriate by B&S. Ensure that final plans are consistent with the measure.	-At site inspection.	-At least once, as required.				
<p>GEO-2 Liquefaction Studies. Prior to construction of new development within the Specific Plan area, site-specific geologic and soils studies shall be performed. The studies shall include site-specific depth to groundwater and soil composition identification, with minimum boring depths as set forth in CDMG 1997 (California Department of Conservation, Division of Mines and Geology, 1997, Guidelines for Evaluating and Mitigating Seismic Hazards in California, Special Publication 117). Areas having liquefiable sediments shall be identified, and structures shall be properly designed to Uniform Building Code and California Building Code standards to withstand the conditions. Such studies shall be conducted and submitted for review and approval by the City prior to issuance of a Grading Permit.</p> <p>Suitable measures to reduce liquefaction include, but are not limited to:</p>	Ensure that, where required, geologic and soils studies are consistent with the mitigation measure, and that development adheres to study recommendations as well as to the CBC as deemed appropriate by B&S. Ensure that studies are adequate and that final plans are consistent with the measure.	<p>-Prior to approval of grading permits for future projects.</p> <p>-At site inspection.</p>	<p>-Once per project application.</p> <p>-At least once, as required.</p>	<p>PCD</p> <p>B&S</p>			

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<ul style="list-style-type: none"> • Specialized design of foundations by a structural engineer; • Removal or treatment of liquefiable soils to reduce the potential for liquefaction; • In-situ densification of soils; • Other alterations to ground characteristics. 							
<p>GEO-3(a) Geotechnical Evaluation. Individual developments shall provide site-specific geotechnical evaluations and geological reports that address onsite soils and slope stability hazards as part of the initial application process. Prior to approval of a specific development plan, these studies shall be submitted to the City Planning and Community Development Department and/or consultants hired by the City for review and approval as part of the initial application process. These evaluations shall determine the potential for adverse soil stability impacts and shall identify appropriate mitigation techniques. All mitigation recommendations identified in site-specific studies shall be implemented as a condition of future development. Such measures may include avoidance of development in areas found to have unmitigable soil or geologic hazards, soil or grading modifications to ensure acceptable slope stability on manufactured slopes, structural measures to ensure slope stability, drainage control facilities to collect and direct water off of slopes, removal of loose cobbles and boulders from adjacent slopes, and/or other measures deemed appropriate to ensure proper slope stability. If site-specific geologic mitigation measures are found to cause secondary environmental effects not addressed herein (excessive import or export of soil material, retaining walls, blasting, etc.), subsequent environmental analysis, may be required.</p>	<p>Ensure that, where required, site-specific geotechnical evaluations and geological reports address onsite soils and slope stability hazards, as well as other factors, and appropriate means of mitigating hazards. Ensure that development adheres to recommendations of such studies as deemed appropriate by B&S. Ensure that studies as well as final plans are reviewed and found consistent with the measure. Ensure that secondary effects of mitigation are also addressed, including subsequent environmental review if warranted.</p>	<p>-Prior to approval of permits for future projects</p> <p>-At site inspection.</p>	<p>-Once per project application.</p> <p>-At least once, as required.</p>	<p>PCD</p> <p>B&S</p>			

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<p>GEO-3(b) Erosion Control Plan. A site-specific erosion control plan that incorporates best management practices shall be prepared by individual applicants and approved by the City prior to the granting of any grading permits for an individual development within the project area. Measures identified in such plans shall be implemented. Such measures may include slope protection measures, netting and sandbagging, landscaping and possibly hydroseeding, temporary drainage control facilities such as retention areas, etc. Landscaping shall be designed by a licensed landscape architect with final landscaping plans to be reviewed and approved by the City Building Official prior to project approval.</p>	<p>Require submittal and implementation of site-specific erosion control plans for future projects. Ensure that landscape plans are prepared by a licensed landscape architect and that final landscaping plans are reviewed and approved by the City Building Official.</p>	<p>-Prior to approval of permits for future projects</p> <p>-At site inspection.</p>	<p>-Once per project application.</p> <p>-At least once, as required.</p>	<p>PCD</p> <p>B&S</p>			
<p>GEO-3(c) City Oversight and Approval. The City Engineer or equivalent shall inspect a project after the final grading report has been filed. The project shall not be approved for construction by the City Engineer or equivalent until all hazards either caused by project grading or associated with adjoining geologic and soils conditions, such as erosion and slope instability, are mitigated to the City's specifications.</p>	<p>Inspect development sites after filing of final grading report. Ensure that building permits are not issued until all hazards as specified in the measure are mitigated.</p>	<p>After the final grading report has been filed and prior to issuance of building permits for future projects.</p>	<p>Once per project application.</p>	<p>PCD</p> <p>B&S</p>			
<p>GEO-4(a) Test Blast/Vibration Study & Blasting Plan. If a site-specific geologic, geotechnical, or structural design study deems blasting necessary for grading and excavation onsite, the applicant must perform a test blast/vibration study to evaluate the variation in vibratory ground motion intensity with respect to distance from the blast site. It must be shown that the blasting can be done safely with respect to existing improvements.</p> <p>A blasting plan shall be provided as part of the vibration</p>	<p>Require test blast/vibration studies as part of the initial application submittal to PCD for applicable future projects, consistent with the standards in the mitigation measure. The City Council and Fire Marshall, in addition to PCD, shall review and approve</p>	<p>With initial application / prior to approval of permits for future projects.</p>	<p>Once per project application.</p>	<p>PCD</p> <p>B&S</p>			

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study, and submitted as part of the initial application submittal to the City Planning and Community Development Department, City Council and Fire Marshall for approval. Blasting permit approval would be subject to the City's discretion and may be denied. If the City were to approve the blasting plan, at a minimum it should be designed to minimize ground shaking away from the blast area. Any areas having unstable slopes or rockfall hazards shall be secured to prevent injury or property damage. If approved, the permittee shall provide sufficient supervisory control as determined by the building official during the grading operation to ensure compliance with approved plans and with the municipal code. When found necessary by the City Building Official, the permittee shall employ a qualified geologist and foundation engineer to assist in supervising the grading operation. If a blasting permit is denied by the City, the applicant shall prepare an alternative application for development which excludes the need for blasting.	blasting plans.						
<p>GEO-5(a) Foundations and Project Infrastructure Design. As provided in mitigation measure GEO-3(a), a site specific geotechnical evaluation shall be conducted for individual projects and submitted to the City Planning and Community Development Department for review and approval as part of the initial application. If the project site is identified to be in a high expansive soil zone based on the site specific Geotechnical Investigation, the foundations and project infrastructure shall be designed by a structural engineer to withstand the existing conditions or the site shall be graded in such a manner as to address the condition.</p> <p>Suitable measures to reduce impacts from expansive soils could include one or more of the following</p>	Require that, for projects in a high expansive soil zone, the foundations and project infrastructure are designed by a structural engineer to withstand the existing conditions, or that the site is graded in such a manner as to address the condition.	<p>-Prior to approval of future projects</p> <p>-At site inspection.</p>	<p>-Once per project application.</p> <p>-At least once, as required.</p>	<p>PCD</p> <p>B&S</p>			

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techniques, as determined by a qualified geotechnical engineer: <ul style="list-style-type: none"> • excavation of existing soils and importation of non-expansive soils; and • foundation design to accommodate certain amounts of differential expansion such as posttensional slab and/or ribbed foundations designed in accordance with Chapter 18, Division III of the UBC; imported fill shall be tested to ensure it is suitable to be used as fill. 							
GEO-5(b) Soils and Foundation Report. To avoid soil-related hazards, the individual project applicants shall provide a soils/foundation report as part of the initial project application to the City Planning and Community Development Department (standard requirement).	Require soils/foundation reports as part of the initial application submittal to PCD for applicable future projects.	With initial application.	Once per project application.	PCD			
GEO-6(a) Settlement Related Mitigation. Future development shall comply with all recommendations contained in site-specific geologic, geotechnical, and structural design studies as required to be prepared for subsequent development activities. Subsequent subsurface investigations shall determine the required degree of compaction and the proper moisture content and appropriate means of mitigating settlement related hazards. Recommendations contained in these site-specific studies shall be reviewed and approved by the City Planning and Community Development Department and City Building Official and incorporated into final grading and structural design plans, as deemed appropriate by the City Building Official prior to issuance of a Grading Permit and/or Building Permit. At a minimum, suitable measures to reduce settlement impacts shall include, but not be limited to:	Ensure that future development complies with all recommendations contained in site-specific geologic, geotechnical, and structural design studies. Ensure that studies as well as final plans are reviewed and approved by PCD and B&S and found consistent with the measure.	-Prior to approval of future projects -At site inspection.	-Once per project application. -At least once, as required.	PCD B&S			

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<ul style="list-style-type: none"> • Removal of organic material in the area of the proposed grading • Removal of non-engineered artificial fill in areas to receive engineered fill or in areas where structural support is required • Placement of a keyway at the bottom of all fill slopes a minimum depth of 3 feet and down to the bedrock with the keyway a minimum of 10 feet wide (unless otherwise determined by the site-specific geological study) • Fill soils shall be benched into the hillside • Removal of upper soils to the bedrock <p>After excavation:</p> <ul style="list-style-type: none"> • All bottoms of the excavations and areas to receive slabs shall be scarified and compacted to 90% • All fills and backfills should be placed in horizontal layers less than 8 inches in loose thickness • Soils shall be compacted to a minimum of 90% of the maximum density rendered by the latest ASTM version • Moisture content should not vary more than 2% from the optimum moisture content, although the grading process will be more easily accomplished with the soils being 1 – 2 % wetter than optimum moisture content • Any utility trenches will need to be properly backfilled as detailed above • Any import soils should be approved by a qualified geologist • Slope faces shall be compacted to at least 90% of maximum compaction 							

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GEO-6(b) Additional Environmental Review. If individual developers are unable to find a disposal site for construction cut within 12.5 miles of the Specific Plan area, or if processed soil is not suitable for fill, then individual projects may require additional environmental analysis. Individual developers must demonstrate a means for disposal of excess cut materials, within 12.5 miles of the project site, prior to approval by the City.	Require developers to demonstrate a means for disposal of excess cut materials, within 12.5 miles of the project site, prior to approval by the City. Ensure that additional environmental analysis is performed in circumstances specified in the measure.	-Prior to approval of future projects -At site inspection.	-Once per project application. -At least once, as required.	PCD B&S			
HAZARDOUS MATERIALS							
HAZ-3 Phase I ESA. As part of the initial project application submittal for a new project or for revitalization of an existing development, a project applicant shall be required to prepare a Phase I Environmental Site Assessment (ESA) to examine the potential for onsite contamination issues. For redevelopment of existing structures, the Phase I ESA shall include examination of the possible presence of asbestos containing materials and lead based paint. In the event that recognized adverse environmental conditions are identified, additional Phase II environmental testing shall be performed and recommended mitigation requirements implemented. If necessary, remediation activities (i.e. excavation and removal of contaminated soils, vapor extraction, removal of contamination source) shall be performed under the supervision of a lead oversight agency to be determined based on the nature of the issue identified. If remediation activities are required, the lead oversight agency shall provide confirmation to the City that onsite environmental issues have been mitigated to a level that is suitable for the anticipated site	Require Phase I ESAs as part of the initial application submittal to PCD for applicable future projects, with scopes as defined in the measure. Require Phase II ESAs where recommended by the results of the Phase I. Ensure that appropriate remediation is carried when required to the satisfaction of the specific oversight agency by obtaining confirmation from the agency.	-With initial application -Prior to approval of permits for future projects.	-Once per project application. -Once per project application.	PCD			

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use or reuse.							
HISTORIC AND ARCHAEOLOGICAL RESOURCES							
<p>HA-1(a) Protection of Known Cultural Resources. Prior to development, as part of the initial project application, a qualified archaeologist and Native American Monitor shall make a reasonable effort to identify archaeological resources from known archaeological sites (as listed in EIR Section 4.6.1.b) within the project area. If it can be demonstrated that a project will cause damage to a unique archaeological resource, a reasonable effort shall be made to permit any or all of these resources to be preserved in place or left in an undisturbed state. As part of the applicant's initial project application, the preferred method of protection/treatment shall be submitted to the City's Community Development Department for review and approval. Examples of that treatment, in no order of preference, may include, but are not limited to, the following:</p> <ul style="list-style-type: none"> • Planning construction to avoid archaeological sites where feasible. • Deeding archaeological sites into permanent conservation easements. • Planning parks, greenspace, or other open space to incorporate archaeological sites. • Dedication of informational booth which explains Native American cultural heritage and displays recovered artifacts from the project site. • Salvage and recordation of resources by a qualified archaeologist. These resources shall be preserved onsite in an interpretive center, designed under the review of both the Native American Heritage 	<p>Require a reasonable effort to identify known archaeological resources as part of the initial application submittal to PCD for applicable future projects.</p> <p>Ensure that applications include protection/treatment measures when warranted as described in the mitigation measure.</p> <p>Ensure compliance with the requirements of California Public Resources Code 21083.2.c.</p>	<p>With initial application / prior to approval of permits for future projects.</p>	<p>Twice per project application.</p>	<p>PCD</p>			

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<p>Commission and the City of Agoura Hills. Pursuant to Public Resources Code 21083.2.c., the project applicant shall provide a guarantee to the lead agency to pay one-half the estimated cost of mitigating the significant effects of the project on unique archaeological resources. In determining payment, the lead agency shall give due consideration to the in-kind value of project design or expenditures that are intended to permit any or all archaeological resources or California Native American culturally significant sites to be preserved in place or left in an undisturbed state. When a final decision is made to carry out or approve the project, the lead agency shall, if necessary, reduce the specified mitigation measures to those which can be funded with the money guaranteed by the project applicant plus the money voluntarily guaranteed by any other person or persons for those mitigation purposes. In order to allow time for interested persons to provide the funding guarantee referred to in this subdivision, a final decision to carry out or approve a project shall not occur sooner than 60 days after completion of this environmental impact report. For time and cost limitations refer to 21083.2(e).</p>							
<p>HA-1(b) Construction Monitoring. Initial grading activities near archaeological sites CA-LAN-1436, CA-LAN-1352, and CA-LAN-41 shall be monitored by a qualified archaeologist and Native American Monitor. If cultural resource remains are encountered during construction or land modification activities, the applicable procedures established under CEQA (CEQA Guidelines §15064.5). In this event the City's Department of Planning and Community Development shall be notified at once and work shall stop within a 100 ft radius until a qualified archaeologist satisfactory to the City has</p>	<p>Require construction monitoring as specified in the measure for grading near the identified known sites.</p> <p>Ensure CEQA and City guidelines and the standards in the measure are followed if cultural resource remains are</p>	<p>-During grading for future projects</p> <p>-At site inspection.</p>	<p>-Once.</p> <p>-At least once, as required.</p>	PCD			

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assessed the nature, extent, and potential significance of any cultural remains. If such remains are determined to be significant, appropriate actions to mitigate impacts to the remains shall be implemented per Section 21083.2 of the Public Resources Code. Depending upon the nature of the find, mitigation could involve avoidance, documentation, or other appropriate actions, to be determined by a qualified archaeologist.	encountered during grading. Ensure compliance with the requirements of California Public Resources Code 21083.2.c.						
HA -1(c) Archaeological Discovery. If human remains are unearthed, State Health and Safety Code Section 7050.5 requires that no further disturbance shall occur until the County Coroner has made the necessary findings as to origin and disposition pursuant to Public Resources Code Section 5097.98. If the remains are determined to be of Native American descent, the coroner has 24 hours to notify the Native American Heritage Commission (NAHC). The NAHC will then identify the person(s) thought to be the Most Likely Descendent (MLD) of the deceased Native American, who will then help determine what course of action should be taken in dealing with the remains.	Ensure compliance with State Health and Safety Code Section 7050.5 and California Public Resources Code Section 5097.98.	During grading for future projects.	Once per project.	PCD			
HYDROLOGY AND WATER QUALITY							
HYD 2 Final Drainage Plans. Individual project applicants shall be required to prepare and submit a final drainage plan, prior to issuance of a grading permit, to the City's Planning and Community Development Department and Los Angeles County Flood Control for approval. Plans shall include detailed design and hydraulic analysis of the drainage facilities that capture and convey on- and off-site runoff. Each developer shall be required to evaluate the extent of potential flood hazards present utilizing the Modified Rational Method	Require submittal of a final drainage plan, consistent with the measure, to PCD and Los Angeles County Flood Control prior to issuance of a grading permit. Ensure that any mitigation meets all interim peak flow	-Prior to issuance of a grading permit for future projects -At site inspection.	-Once per project application. -At least once, as required.	PCD			

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(or the latest model approved by Los Angeles County Flood Control) and to implement mitigation measures required to reduce such impacts to a level of insignificance. The drainage plan for each project shall include post development designs for runoff detention and on site infiltration to reduce 50-year frequency storm peak discharge to the pre development level. These drainage facilities shall meet the design requirements and capacities of the Master Plan of Drainage for the City of Agoura Hills, The Los Angeles County Department of Public Works Hydrology Manual and the Hydrology and Sedimentation Appendix, or other revised hydraulic analyses as determined by the City Engineer, and shall not increase the base flood elevation above or below the project site. Additionally, mitigation shall meet all interim peak flow standards, or the most up to date standards, as established by the LACDPW. The plans shall be subject to review and approval by the City Engineer.	standards, or the most up to date standards as established by the LACDPW and that the plans are reviewed and approved by the City Engineer.						
HYD-3(a) Hydrology Study. If any onsite open channels are altered, a channel bed erosion study shall be conducted as part of a hydrology report submitted to the City as part of the initial application submittal. The erosion study shall determine if additional grade stabilization structures are necessary for any restored areas within Medea Creek or within Lindero Canyon Creek. Recommendations of this study shall be fully implemented subject to review and approval by the City of Agoura Hills and Los Angeles County Public Works Department. Design of modifications to Medea Creek shall meet the standards of the City of Agoura Hills and Los Angeles County Public Works Department, and shall be approved by the City prior to the issuance of grading permits.	Require that a channel bed erosion study, designed as specified in the measure, is part of hydrology reports in initial application submittals of any onsite open channels are altered. Ensure that recommendations of the study are fully implemented subject to review and approval by the City and the Los Angeles County Public Works Department	With initial application / prior to issuance of grading permits for future projects.	Once per project application.	PCD			

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<p>HYD-3(b) Public Facilities Flood Protection. Any trunk sewer manholes located adjacent to Lindero Canyon Creek and Medea Creek shall be protected from peak flows laden with debris by further armoring via cement casing, piercing, or other appropriate method. A plan to protect the sewerline and exposed manholes from erosion and flooding and from construction activity shall be submitted to the Las Virgenes Municipal Water District for review, comment, and approval prior to the issuance of grading or building permits.</p>	<p>Require a plan to protect the sewerline and exposed manholes as specified in the measure for projects adjacent to the identified creeks.</p> <p>Ensure review and approval by the Las Virgenes Municipal Water District, as well as the City, prior to the issuance of grading or building permits.</p>	<p>Prior to issuance of grading permits for future projects.</p>	<p>Once per project application.</p>	<p>PCD</p>			
NOISE							
<p>N-1 Construction Hours. On-site construction activity, including blasting, or involving the use of equipment or machinery that generates noise levels in excess of the 55 dBA standard shall be limited to between the hours of 7 AM and 8 PM, Monday through Saturday pursuant to City Ordinance 9656 and City Municipal Code Section 9666.4. No construction activity shall occur between 8 PM and 7AM that generates noise in excess of the 50 dBA standard. No construction activity shall take place on Sundays or legal holidays.</p>	<p>Require that project construction schedules adhere to the days, hours and limitations expressed in the condition.</p>	<p>-Prior to approval of future projects</p> <p>-At site inspection.</p>	<p>-Once per project application.</p> <p>-At least once, as required.</p>	<p>PCD</p>			
<p>N-2(a) Rubberized Asphalt. In potentially noise impacted areas within the Specific Plan, the City shall consider and, if feasible, use rubberized asphalt paving material for street re-paving projects. Studies have demonstrated that this type of paving materials can substantially reduce roadway noise. A 1992 noise study in the City of Thousand Oaks by Acoustical Analysis</p>	<p>Ensure that, where applicable, rubberized asphalt paving material is used for street re-paving projects.</p>	<p>-Prior to approval of future projects</p> <p>-At site inspection.</p>	<p>-Once per project application.</p> <p>-At least once, as required.</p>	<p>PCD</p>			

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Associates, Inc. indicated that the use of an asphalt rubber overlay can achieve a noise reduction of from 2 to 5 dBA as compared to standard asphalt.							
N-2(b) Sound Wall. If traffic-related noise problems from U.S. 101 arise within the Specific Plan area, the City shall investigate and, if feasible, implement appropriate measures to reduce noise impacts at affected receptor locations. Such measures may include, but are not limited to, the use of a sound wall along the northern boundary of the Specific Plan area, between Roadside Drive and U.S. 101. It is estimated that a 10-foot high sound wall located adjacent to the southern edge of U.S. 101 would decrease noise levels at the property boundaries on the southern side of Roadside Drive from 78.8 dBA to 69.3 dBA (refer to Appendix E for Sound Barrier Loss Estimation Spread Sheet).	Investigate and, if feasible, implement appropriate measures, which could include a sound wall along Roadside Drive, to reduce noise impacts from Highway 101 at affected receptor locations.	If/when traffic-related noise problems from U.S. 101 arise within the Specific Plan area.	At least once depending on results of initial action.	PCD			
N-3(a) Acoustical Study. A site-specific acoustical study shall be submitted to the City Planning and Community Development Department as part of the initial application for any residential project located within the project area that is exposed to freeway or arterial traffic noise. This study shall contain specific structural and site design recommendations to be incorporated into the project design to mitigate any noise levels that exceed the City's residential exterior standard of 65 CNEL.	Require site-specific acoustical study as part of the initial application for any residential project located within the project area that is exposed to freeway or arterial traffic noise, pursuant to the standards listed.	With initial application / prior to approval of permits for future projects -At site inspection	Twice. -At least once, as required.	PCD			
N-3(b) Operating Hours. Loading dock and delivery truck (i.e. refrigerator trucks, trash and recycling pickups) and parking lot sweeping hours shall be restricted to daytime operating hours (7:00 AM to 7:00 PM). Delivery trucks entering and leaving the site shall not block driveways and shall be allowed to idle no more than 15 minutes in any half hour period.	Require that proposed loading, delivery and parking lot sweeping activities for future projects adhere to the hours and standards specified.	-Prior to approval of future projects -At site inspection.	-Once per project application. -At least once, as required.	PCD			

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N-3(c) Loading Dock Location. To the degree feasible, loading docks and delivery areas shall be located out of line of sight and/or oriented away from nearby residences.	Require that proposed loading docks and delivery areas for future projects adhere to the standards specified.	-Prior to approval of future projects -At site inspection.	-Once per project application. -At least once, as required.	PCD			
N-3(d) Ventilation Noise. Parapets that reduce noise from rooftop ventilation systems shall be installed on all project structures.	Require that projects with rooftop ventilation systems include noise-reducing parapets.	-Prior to approval of future projects -At site inspection.	-Once per project application. -At least once, as required.	PCD			
N-3(e) Parking Lot Noise. Surface-texturing materials and landscaping shrubs and trees shall be used in the parking areas to reduce parking lot related noise.	Require that proposed parking lots include the specified features.	-Prior to approval of future projects -At site inspection.	-Once per project application. -At least once, as required.	PCD			
N-3(f) Mechanical Equipment. All exterior mechanical equipment shall be oriented away from adjacent residential uses and shall be fitted with sound-rated parapets.	Require exterior mechanical equipment to be oriented away from adjacent residential uses and fitted with sound-rated parapets.	-Prior to approval of future projects -At site inspection.	-Once per project application. -At least once, as required.	PCD			
N-3(g) Interior Noise. At a minimum, all on-site structures shall include the following or equivalent to achieve an acceptable interior noise level of 45 CNEL: <ul style="list-style-type: none"> • Air conditioning or a mechanical ventilation system so that windows and doors may remain closed • Double-paned windows and sliding glass doors mounted in low air infiltration rate frames (0.5 cubic feet per minute, per ANSI specifications) • Solid core exterior doors with perimeter weather 	Ensure that proposed structures include the listed items to reduce interior noise below 45 CNEL.	-Prior to approval of future projects -At site inspection.	-Once per project application. -At least once, as required.	PCD			

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stripping and threshold seals <ul style="list-style-type: none"> Roof and attic vents facing away from Highway 101 Incorporation of these design requirements would be expected to achieve an interior noise level reduction of 25 dB or greater.							
PUBLIC SERVICES							
PS-3(a) Fuel Modification Plan (FMP). Individual project applicants shall develop a Fuel Modification Plan for all development areas within or adjacent to wildland fire hazard areas. These plans shall be subject to review and approval by the Los Angeles County Fire Department Fuel Modification Unit. The FMP shall be submitted to the City Planning and Community Development Department for approval prior to issuance of a grading or building permit. Funding and execution of all measures required in the FMP shall be the responsibility of individual developers or land owners. Prior to approval of the FMP the City shall confirm that appropriate easements have been secured and that long-term funding mechanisms are in place to ensure successful implementation of the FMP.	Require Fuel Modification Plans for proposed development within or adjacent to wildland fire hazard areas. Ensure review and approval by the Los Angeles County Fire Department Fuel Modification Unit.	-Prior to issuance of a grading or building permit. -At site inspection.	-Once per project application. -At least once, as required.	PCD			
PS-3(b) Landscape Palette. The landscape palette for the project shall prohibit the use of highly flammable species near areas of open space.	Ensure that landscape plans prohibit the use of highly flammable vegetation near open space areas.	-Prior to approval of permits for future projects -At site inspection.	-Once per project application. -At least once, as required.	PCD			
PS-3(c) Roundabout Engineering. Further detailed	City to ensure that detailed	-Prior to	-Once for the	PCD			

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<p>engineering design shall be performed for the proposed roundabout at the intersection of Agoura Road and Kanan Road. The engineering design shall incorporate the applicable geometric features required to accommodate the forecast vehicular, bicycle and pedestrian movements, and safety personnel/emergency access. The engineering design shall incorporate the appropriate capacity and safety elements at the roundabout for both pedestrians and motorists.</p>	<p>engineering design is completed for the identified public improvement, consistent with the measure.</p> <p>Six months after completion of the roundabout, the City shall commission post – construction monitoring of operational and safety characteristics of the roundabout. This monitoring shall include but not be limited to: Monitoring of vehicle flows, delays and queuing to determine LOS; and the monitoring and assessment of pedestrian and bicycle movements during peak periods. The results of the monitoring shall be reported to the City Council as soon as practical after completion of the program. Upon review of the monitoring report, the City Council shall determine if additional monitoring is necessary and, if so, the scope and the frequency of such monitoring.</p>	<p>finalization of roundabout design.</p> <p>-Monitoring to occur once following completion of the roundabout, or as otherwise directed by City Council following review of the initial monitoring results.</p>	<p>design studies.</p> <p>-To be determined based on the results of initial monitoring.</p>				
<p>PS-3(d) Emergency Access. The proposed roundabout at the intersection of Kanan and Agoura</p>	<p>City to provide public education materials, such</p>	<p>Prior to finalization of</p>	<p>Once.</p>	<p>PCD</p>			

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Road has the potential to restrict access to safety personnel and emergency vehicles. Public education should include information on driver behavior in the event of an emergency vehicle, which is similar to the driver behavior required at conventional intersections. All approaches to the roundabout shall contain two lanes. Vehicles in queue in front of an emergency vehicle would either move to another lane or move through the roundabout to facilitate passage of the emergency vehicle. The design of the roundabout shall include a mountable apron on the island and mountable splitter islands. In the event of blockage of the circulatory roadway, these elements would provide for sufficient width within the roundabout for passage of emergency vehicles.	as signs near approach of roundabout, (other education materials may include an online tutorial). Additionally, the City shall review design and installation of mountable apron and splinter islands.	roundabout design.					
PS-4(a) Design Approval. Project plans shall be submitted to the Los Angeles County Sheriff's Department Lost Hills Substation for review and comment. All recommendations made by the Department, including, but not limited to, those pertaining to site access, site security, lighting, and requirements for onsite security, shall be incorporated into the design of the project, prior to approval of final building permits.	City to ensure LA County Sheriff's Department review of project plans consistent with the measure, and that the Sheriff's comments be incorporated into the project.	Prior to project approvals.	Once.	PCD			
PS-4(b) Roundabout Engineering. Refer to Mitigation Measure PS-3(c). This measure shall also be subject to review and approval by the Los Angeles County Sheriff's Department.	City to ensure that detailed engineering design for the Agoura/Kanan roads roundabout is reviewed and approved by the LA County Sheriff's Department.	Prior to finalization of roundabout design.	Once.	PCD			
PS-5(a) In Lieu Fees. Individual project applicants shall pay the statutory school fees in effect at the time of issuance of building permits to the appropriate school districts. If permissible, at the time the application is	Ensure statutory school fees are collected by the School District as required.	Prior to issuance of building permits.	Once per project approval.	PCD			

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processed, these fees shall include additional District costs associated with impacts to student transportation or other measures to alleviate student transportation overcrowding (e.g. pro-rata contribution to new school transportation systems, student carpooling bulletin boards, etc.)							
PS-5(b) School District Noticing. The applicant shall notify the Las Virgenes Unified School District of the expected buildout date of the project as soon as possible to allow the District to plan in advance for new students.	Ensure that applicants notify the Las Virgenes Unified School District of the expected buildout date of their projects.	Prior to issuance of building permits.	Once per project approval.	PCD			
TRANSPORTATION AND CIRCULATION							
<p>T-2(a) Kanan Road/Canwood Street - U.S. 101 Northbound Ramps intersection (A.M. and P.M. peak hour): Additional capacity will need to be provided at this intersection to obtain acceptable operations. As part of the Kanan Interchange Projects, the future geometry for the southbound approach of the intersection includes three southbound through lanes and a separate right-turn lane. One southbound through lane is a trap lane onto the Northbound On-Ramp, and two through lanes would continue onto the overpass.</p> <p>Future cumulative peak hour volumes on the southbound through approach would exceed 2,000 vehicles per hour (vhp) during the A.M. peak hour and would exceed 1,700 vph during the P.M. peak hour. These volumes indicate the need for additional southbound capacity.</p> <p>Additional measures that would be necessary include restriping of the southbound approach to three through lanes and a shared through/right –turn lane would improve the intersection operations to LOS D during the A.M. peak hour and LOS C during the P.M. peak hour.</p>	Ensure that funding is secured and the specified improvements are implemented.	After plan adoption as individual projects are proposed.	Ongoing.	PCD			

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<p>This mitigation would require that the Northbound on-ramp approach be moved 16 feet (4.9 m) to the west and the overpass be restriped from two southbound lanes to three southbound lanes. The southbound direction on the overpass contains 43.5 feet (13.3 m), which is sufficient to accommodate three 11.8 feet (3.6 m) wide lanes and a 4 feet (1.2 m) wide bike lane.</p> <p>Additional widening on the eastbound approach (Canwood Street) is required to provide LOS C during the A.M. peak hour. The eastbound approach would need to be widened from one left-turn lane and one right-turn lane to one left-turn lane, a shared left/right-turn lane, and a right-turn lane. The mitigated geometry is shown below and the mitigated levels of service are shown below in Tables 4.11-9 and 4.11-10.</p>							
<p>T-2(b) Palo Camado Canyon Road/U.S. 101 Northbound Ramps intersection (A.M. and P.M. peak hour): City staff have indicated that several improvement options for the intersection are being evaluated as part of the EIR underway for the Heschel West school site proposed east of Palo Camado Canyon Road within County limits. Improvement options that are evaluated include installation of a signal, widening of the overpass and/or intersection approaches, and construction of a roundabout. It is noted that the cumulative traffic forecasts derived from the Agoura Hills Traffic Model did not include any traffic volumes associated with the proposed Heschel West school site.</p> <p>The future evaluation process for the intersection and/or the U.S. 101/Palo Camado Canyon-Dorothy Drive interchange would likely be through the Caltrans process, which would evaluate all future traffic volumes (including</p>	<p>Ensure that funding is secured and the specified improvements are implemented.</p>	<p>After plan adoption as individual projects are proposed.</p>	<p>Ongoing.</p>	<p>PCD</p>			

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the Heschel West school traffic) and mitigation options. It is anticipated that the ultimate intersection and/or interchange improvements would provide for acceptable levels of service at this location during the peak hours. The project would contribute its proportionate share to any improvement that will be elected for this intersection.							
T-2(c) Reyes Adobe Road/Canwood Street intersection (P.M. peak hour): The City has programmed the widening of the northbound approach as part of the U.S. 101/Reyes Adobe interchange improvement project. After implementation of the proposed improvements, the intersection would operate at LOS A during the P.M. peak hour, thereby reducing the project's impact to a level of insignificance. It is noted that no implementation schedule has been developed for this project at this time. (The mitigated level of service is shown in the EIR in Table 4.11-10.)	None required.	n/a	n/a	n/a			
T-2(d) Reyes Adobe Road/U.S. 101 Southbound Ramps intersection (P.M. peak hour): The City has programmed the widening of this intersection as part of the U.S. 101/Reyes Adobe interchange improvement project. After construction, the intersection would operate at LOS C during the P.M. peak hour, thereby reducing the project's effect to less than significant. It is noted that no implementation schedule has been developed for this project at this time. The mitigated level of service is shown above in Table 4.11-10.	None required.	n/a	n/a	n/a			
T-2(e) Reyes Adobe Road/Agoura Road intersection (P.M. peak hour): Restriping the southbound approach to provide dual left-turn lanes and a right-turn lane, and providing additional capacity on the westbound approach would result in LOS C during the P.M. peak hour, thereby	Ensure that funding is secured and the specified improvements are implemented.	After plan adoption as individual projects are proposed.	Ongoing.	PCD			

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reducing the project's impact to less than significant. There are two receiving lanes on all three legs of this intersection. The southbound approach contains one left-turn lane and the right-turn lane which are separated by a wide striped channelization island. There is sufficient pavement width between the raised median and the western curb (43 ft) to restripe the approach to two left-turn lanes and a right-turn lane. In addition, the westbound approach should be restriped to a shared through/right-turn lane and a dedicated right-turn lane, or be widened to include an additional lane (through, through-right, and right-turn lane) to provide LOS C during the P.M. peak hour. The mitigated level of service is shown in the EIR in Table 4.11-10.							
T-2(f) Kanan Road/Canwood Street (E) intersection (P.M. peak hour): This intersection was recently reconstructed as part of the Kanan Road/U.S. 101 interchange improvement project. Kanan Road contains two northbound through lanes and a right-turn lane; the southbound approach contains a left-turn lane and three through lanes. A third northbound through lane (two through lanes and a through-right-turn lane) is required to provide LOS C during the P.M. peak hour. This mitigation measure would require some widening of the north side of the intersection for 200 ft or more to provide three receiving lanes. The mitigated level of service is shown in the EIR in Table 4.11-10.	Ensure that funding is secured and the specified improvements are implemented.	After plan adoption as individual projects are proposed.	Ongoing.	PCD			
T-2(g) Kanan Road/Roadside Drive - U.S. 101 Southbound Ramps intersection (P.M. peak hour): Additional capacity on the northbound and southbound approaches will need to be provided at this intersection to provide LOS C operations. The required improvements are outlined below:	Ensure that funding is secured and the specified improvements are implemented.	After plan adoption as individual projects are proposed.	Ongoing.	PCD			

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<p>There are three northbound receiving lanes provided on the north side of the intersection. Under the proposed intersection design, two lanes continue onto the overpass and one lane traps into the U.S. 101 Southbound On-Ramp. The northbound approach would contain one through lane and one shared through/right-turn lane. This approach should be widened to provide two through lanes and one shared through/right-turn lane.</p> <p>Under the proposed intersection design, the southbound approach would contain one left-turn lane, two through lanes and one right-turn lane. To provide LOS C during the P.M. peak hour, a second southbound left-turn lane is needed. There is sufficient roadway width provided on the north leg of the intersection and the overpass to provide dual left-turn lanes, two through lanes and a right-turn lane on the southbound approach, and retain the three northbound receiving lanes provided on the north side of the intersection. The bike lane on the southbound approach shown on the proposed intersection design may need to be eliminated. It is noted that the lane widths on the north leg (11-foot left-turn lanes, 11-foot through lanes and 12 to 13-foot right-turn lanes) would be less than the lane widths specified by Caltrans (12-foot left-turn lanes, 12-foot through lanes and 16-foot right-turn lanes), and would require approval of a design exception.</p> <p>Additionally, the east leg of the intersection (Roadside Drive) would need to be widened to the south to provide two receiving lanes.</p> <p>Implementation of the above improvements would result in LOS C (V/C 0.78). The mitigated geometry is shown below followed by the mitigated level of service as shown</p>							

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							in Table 4.11-10.
							<p>T-2(h) Dorothy Drive/U.S. 101 Southbound Ramps Intersection (P.M. peak hour): This intersection is currently controlled by stop signs on all approaches. Signalizing this intersection would result in LOS C during the P.M. peak hour, therefore mitigating the project's impact to a level of insignificance. The mitigated levels of service are shown in the EIR in Table 4.11-10.</p>
						<p>Ensure that funding is secured and the specified improvements are implemented.</p> <p>After plan adoption as individual projects are proposed.</p>	<p>T-3(a) Roundabout Engineering. Refer to Mitigation Measure PS-3(c) in Section 4.10, Public Services.</p>
							<p>T-3(b) Agoura Road/Zone A Pedestrian Crossing. It is recommended that the final design of any intersection at the mid-block of Agoura Road (between Kanam and Cornell Road), if proposed, be configured as a roundabout or a conventional intersection. It should be designed to accommodate pedestrians, bicyclists, and vehicles such as trucks, buses and emergency vehicles to pass through the intersection.</p>
							<p>T-3(c) Pedestrian Friendly Median. As the use of mid-block crosswalks may create safety issues for pedestrians, the median proposed along Agoura Road should also be designed to provide a refuge area for pedestrians using the proposed crossings on Agoura Road. Consideration should be given to making the area more pedestrian friendly.</p>
							<p>T-3(d) Pedestrian Cross Walks. Pedestrian crosswalks should utilize textured and colored surface</p>
							<p>T-3(a) Roundabout Engineering. Refer to Mitigation Measure PS-3(c) in Section 4.10, Public Services.</p>
							<p>T-3(b) Agoura Road/Zone A Pedestrian Crossing. It is recommended that the final design of any intersection at the mid-block of Agoura Road (between Kanam and Cornell Road), if proposed, be configured as a roundabout or a conventional intersection. It should be designed to accommodate pedestrians, bicyclists, and vehicles such as trucks, buses and emergency vehicles to pass through the intersection.</p>
							<p>T-3(c) Pedestrian Friendly Median. As the use of mid-block crosswalks may create safety issues for pedestrians, the median proposed along Agoura Road should also be designed to provide a refuge area for pedestrians using the proposed crossings on Agoura Road. Consideration should be given to making the area more pedestrian friendly.</p>
							<p>T-3(d) Pedestrian Cross Walks. Pedestrian crosswalks should utilize textured and colored surface</p>

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treatments to clearly distinguish these areas for pedestrian movement. Final design must be approved by the City Engineer.	for public improvements.						
T-3(e) Individual Access. The design and control of individual access driveways will need to be determined as individual projects are analyzed. Analysis of these individual access driveways should give consideration to traffic volumes to and from each individual site within the Specific Plan and opposing traffic volumes on the adjacent roadway system.	Ensure that design of individual driveways gives consideration to traffic volumes and patterns consistent with the measure.	Prior to approval of future projects.	Once.	PCD			
T-3(f) Construction Impacts. Prior to individual project approval, short-term construction impacts shall be examined. Where necessary, a construction vehicle management plan shall be developed and implemented. This plan shall include measures to avoid conflicts with nearby businesses and other land uses (such as construction activity notification and timing so as to minimize conflicts) and to minimize the effects on the local street network.	City shall require construction vehicle management plans for projects with potential short-term traffic related construction impacts.	As part of individual project application, prior to approval.	Once.	PCD			

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